

THE LOG



ROYAL ROADS
1959

THE LOG

VOL. 18



CANADIAN SERVICES COLLEGE
Royal Roads

1958 - 1959

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THE LOG

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FOREWORD

It is a pleasure to have an opportunity of saying a word through The Log to the cadets of Royal Roads.

The history of Royal Roads is not long, but the College already has a record to be envied, first as the Royal Canadian Naval College, H.M.C.S. Royal Roads, and then since 1948 as one of the Canadian Services Colleges. During the past decade we in Canada have been engaged in a bold experiment—the training of officers for all three fighting services in the same institutions. Some people, no doubt, said that this could not be done; but the fact is that it has been done and is being done now—and done successfully. Co-operation is one of the Principles of War; the book says, "Goodwill and the desire to co-operate are essential at all levels." This was always true, but it is, if possible, truer today than it ever was before; and the best time for an officer to learn co-operation is at the outset of his military career. You are finding out about it the practical way at Royal Roads.

Every modern officer must be something of a scientist; he must know the nature and the uses of a great variety of complicated equipment. He must also be, in the broader sense, an educated man, who knows how to think and to express his thoughts. You are learning how to do these things. But you are also learning about the spirit of three great fighting forces, each possessing its own traditions. Each was formed by generations of men who worked in different spheres but were alike in the devoted and selfless service they rendered to the Crown and Canada in peace and war. Service of this sort must be the keynote and the inspiration of every worthwhile career in the armed forces. To such a career of service I welcome you; and I wish you luck.

S. J. Clark

Lieutenant-General,
Chief of the General Staff.

EDITORIAL

By C-F/L R. W. Craig

The arduous task of collecting and shaping my thoughts for this editorial has impressed upon me a realization of the importance of learning how to think for oneself. Moreover, I have realized that one of the prime purposes of the program offered at Royal Roads is to train its cadets to think for themselves, in both academic and military areas, so that they may better fulfil the responsibilities which the man undertaking a career in the services must shoulder.

In making a personal assessment of a situation or problem, one must employ his powers of imagination and observation to collect his ideas, as well as his power of reflection and logical reasoning to arrive at the appropriate conclusions which will form a basis for his action. Consider this simplified outline of the process of learning to think for oneself. As a person grows up he is confronted with a seeming infinity of mental challenges. Within his mind various conceptions are formed as he seeks to meet these challenges, his conceptions being more mature as his experience increases. From the time he recognizes and accepts the existence of these problems, the thinking process begins. After many possible solutions and conclusions have been formed, his mind evaluates their practicability. You may recognize this process as similar to what goes on in your mind when you attempt to consider a problem. Since this is a process which most people use many times every day, we can see how important it is for a person to learn to think for himself.

Why is intellectual self-reliance so important? It is important because it enables us to solve the many problems with which we are continually being confronted every day. We must arrive at appropriate conclusions if

we are to cope successfully with the different problems and situations which confront us. Otherwise we could not solve these problems. Thinking for ourselves also enables us to understand people, their actions and feelings. If a person were not able to do this he could not work with and manage other people successfully. Enabling us to solve problems confronting us and enabling us to understand people are two very good reasons why it is important that we learn to think for ourselves. These two points are necessary requirements in almost every walk of life today.

Take, for example, the military profession, for which the young men at Royal Roads are training. At the military colleges this process of thinking for oneself is one of the prime considerations in the training. The entire education is designed to establish this principle and to give a working knowledge of the profession these young men are undertaking. All the reading, discussions, facts presented in class, and problems set before them are presented so as to stimulate this process of thinking. This enables these young men to develop their minds with respect to this process of thinking for themselves and to use them in their training as well as in the job they are training for.

The formation of an independent and effectively functioning mind is a necessary requirement if the cadets are to meet successfully the problems which will undoubtedly face them in their chosen career.

From our brief but worthwhile glance at the importance of learning to think for oneself we can easily see why the possession of a self-reliant mind is necessary in our lives, especially since we are undertaking a career in the services.



"THAT'S A LULU OF A BOO-BOO, BOBO!"

Left to right—Lieut.-Cmdr. T. J. F. Roberts, D. Harris, D. Porter, D. Thomson, Prof. R. F. B. King, R. W. Craig, Prof. R. H. Roy, H. Widditfield, A. D. Hopkins, M. W. Cavanagh, Capt. H. P. Stickley.

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GRADUATES



1959

Prize Winners and Winners of Academy Awards

1959

AWARDS

The Prize of Honour.....	M. W. Cavanagh
The H. E. Sellers' Telescope.....	M. W. Cavanagh
The United Services Institute of Vancouver Island Binoculars.....	R. C. Waller
Nixon Memorial Award.....	R. L. Sykes
The Governor-General's Silver Medal (Second Year).....	V. R. Thomas
The Governor-General's Bronze Medal (First Year).....	J. C. Bauer
The Lieutenant-Governor of Quebec's Medal.....	D. A. Harris
RMC Club (Victoria Branch) Award for Military Training.....	D. A. Thomson
The Commandant's Cup.....	A. W. Pchajek
The Director of Studies' Cup.....	G. L. Walt
The RCAF Association Award.....	A. W. Pchajek

SUBJECT PRIZES

Awarded in each Academic Course to the officer cadet who achieves first place in each subject provided he has obtained First Class Standing in that subject.

SENIOR

English.....	R. H. CRANE
History.....	D. A. HARRIS
Physics.....	V. R. THOMAS
Chemistry.....	V. R. THOMAS
French.....	R. H. THOMAS
Mathematics.....	V. R. THOMAS
Engineering	
Drawing and	
Descriptive	
Geometry.....	E. E. GOSKI
Economics.....	M. B. SULLIVAN

JUNIOR

English.....	J. C. BAUER
History.....	J. C. BAUER
Physics.....	R. C. WALLER
Chemistry.....	F. M. BUCKNUM
French.....	J. C. BAUER
Mathematics.....	J. C. BAUER
Engineering	
Drawing and	
Descriptive	
Geometry.....	E. E. GILLESPIE

MILITARY STUDIES PRIZES

Awarded to the officer cadet in each Service who obtained the highest marks in Military Studies in his graduating year:

R. L. SYKES

J. R. MacDOUGALL

B. D. CLARK

CRESTS

Awarded for Athletic Achievement and Outstanding Sportsmanship:

A. W. PCHAJEK

J. C. PELLOW

R. W. CRAIG

G. L. WALT

GRADUATION, 1959

By C-S/L R. L. Sykes

On September 5, 1957, our graduation seemed a long way off. Looking back now, however, these last two years seem to have flown by. Indeed, until many heard the command "By the right, quick march," as given by CWC Cavanagh, it all seemed like a long dream.

Actually, graduation began on the 18th of May, the day of the Physical Training Demonstration. The sports field was surrounded by proud and anxious parents all patiently awaiting the arrival of their sons. The cadet wing, tense but eager to demonstrate, doubled smartly onto the field and demonstrated to the spectators the numerous athletic skills learned at the college. The demonstration was comprised of a display of Physical Training Exercises and examples of different exercises involving the use of tumbling mats, parallel bars and the box horse. At the conclusion of the display the wing "doubled-past" and carried on smartly up to the Grant Block to shower and change.

Following this display the cadet wing and guests assembled on the lawn in front of the Castle for the awarding of the various academic prizes. Parents and guests were then escorted around the college grounds and through some of the buildings to give those interested a closer look at many of the things which we have been connected with for the last two years. This little tour aided the cadets in telling their stories about the last two years by pointing out various authentic locations. Soon the day was over and all anxiously awaited the next.

May the 19th. The day had arrived. Throughout the morning, while last minute preparations were made, we all tried to look at ease and not tense with excitement and expectations. "Out pipes." The cadet wing fell in on the square and with a final quick-check and a word of advice from CPO Kelly, the cadet wing commander marched the wing to the parade square. All around the square were excited parents and many other curious guests and important dignitaries.

The wing marched on to the parade square. This was not just an ordinary march-on like so many we had done since the fall of 1957, but was the culmination of two years' work, marking the closing of a door behind

us and the beginning of a walk down a new hall. The entire graduating class realized that this was it. The end of one phase of our lives and the beginning of a new phase. The "dressing" was perfect and, so numerous spectators told us later, the march-on was extremely impressive.

The Halt, the Advance in Line, and the rest of the preparatory movements were executed with a skill which comes only from extensive practice. The inspection of the wing was conducted by the Minister of National Defence, the Honorable George Pearkes, and proceeded under the guidance of Cadet Wing Commander Cavanagh and Colonel Cooper.

The March-Past was indeed an exhibition of the peak which we had reached through the past two years of extensive training, discipline and instruction. The Eyes-Right was a tribute and a "thank you" to everyone who had been connected with our training and development over the last two years.

The graduating class then paid a final tribute by marching past in slow time. Compliments were paid and the term marched through the ranks of the new senior term who then presented arms to the graduates. Off the square and up to the block and the parade was over. We had become ex-cadets.

The cadet wing stowed their rifles and gear and moved to the Castle terrace and lawn, where tea was served and pleasant conversation prevailed among the cadets, guests and staff. Soon the crowd began to dwindle as various groups headed towards Victoria to have supper and prepare for the ball.

The Graduation Ball was held in the gymnasium. The juniors had spent many hours decorating and preparing the gym, and their efforts were certainly noticeable and very much appreciated. Most of the cadets and guests had arrived by 2130 hours and everyone settled down to an evening of good fun and dancing. The graduates mingled with the new seniors on a first-name basis and everyone had an enjoyable time dancing to the accompaniment of a section of the Naden Band.

An era had come to a happy and successful close. Our only regret was that so many of those who started with us two falls ago were not with us now to be so richly rewarded.

CADET OFFICERS 1958-1959

FIRST TERM

C-W/C CAVANAGH, M. W.

D-C-W/C GIBBON, L. A.

C-W/A EXLEY, E. E.

No. 1 Squadron

C-S/L Kuntz, R. W.

C-S/A Sykes, R. L.

Cartier Flight

C-F/L Antonsen, P. G.

L/C Dalley, G. W.

L/C Kovacs, R. J.

Fraser Flight

C-F/L Dallimore, G. J.

L/C Gowdy, D. R.

No. 2 Squadron

C-S/L Pellow, J. C.

C-S/A Parkes, K. J.

Champlain Flight

C-F/L Sullivan, M. B.

L/C Thomas, R. H.

Mackenzie Flight

C-F/L Money, S. A.

L/C Grott, M. J.

L/C Macdougall, R. J.

No. 3 Squadron

C-S/L Thompson, D. A.

C-S/A Buchan, J. M.

Hudson Flight

C-F/L Anderson, D. J.

L/C Elder, J. S.

L/C Clark, B. D.

La Salle Flight

C-F/L Pchajek, A. W.

L/C Peter, M. F.

Band

C-B/M Hammond, W. S.

L/C Bernard, J. H.

SECOND TERM

C-W/C THOMPSON, D. A.

D-C-W/C PARKES, K. J.

No. 1 Squadron

C-S/L Sykes, R. L.

C-S/A Gowdy, D. R.

Cartier Flight

C-F/L Dalley, G. W.

L/C Landry, J. A. M.

L/C Kuntz, R. W.

L/C Cavanagh, M. W.

Fraser Flight

C-F/L Saunders, G. A.

L/C Gibbon, L. A.

L/C Lytle, E. R.

No. 2 Squadron

C-S/L Exley, E. E.

C-S/A Hopkins, A. D.

Champlain Flight

C-F/L Smith, D. C.

L/C Takaki, M. W.

L/C Thomas, V. R.

Mackenzie Flight

C-F/L Macdougall, R. J.

L/C Lightburn, D. T.

L/C Pellow, J. C.

No. 3 Squadron

C-S/L Pchajek, A. W.

C-S/A Peter, M. F.

Hudson Flight

C-F/L Clark, B. D.

L/C Goski, E. E.

L/C Widdifield, H. R.

La Salle Flight

C-F/L Craig, R. W.

L/C Fallon, D. E.

L/C Downs, A. T.

Band

C-B/M Bernard, J. H.

THIRD TERM

C-W/C CAVANAGH, M. W.

D-C-W/C THOMPSON, D. A.

C-W/A GOWDY, D. R.

No. 1 Squadron

C-S/L Sykes, R. L.

C-S/A Gibbon, L. A.

Cartier Flight

C-F/L Antonsen, P. G.

L/C Dalley, G. W.

L/C Lytle, E. R.

Fraser Flight

C-F/L Kuntz, R. W.

L/C Saunders, G. A.

L/C Dallimore, G. J.

No. 2 Squadron

C-S/L Pellow, J. C.

C-S/A Parkes, K. J.

Champlain Flight

C-F/L Exley, E.

L/C Smith, D. C.

L/C Thomas, V. R.

Mackenzie Flight

C-F/L Hopkins, A. D.

L/C Lightburn, D. T.

L/C MacDougall, R. J.

No. 3 Squadron

C-S/L Pchajek, A. W.

C-S/A Peter, M. F.

Hudson Flight

C-F/L Anderson, D. J.

L/C Clark, B. D.

L/C Fallon, D. E.

La Salle Flight

C-F/L Craig, R. W.

L/C Downs, A. T.

L/C Widdifield, H. R.

Band

C-B/M Hammond, W. S.

L/C Bernard, J. H.

GRADUATES

**MICHAEL WILLIAM CAVANAGH—No. 4881**

Hamilton, Ontario
Army (Infantry)

Mike, known by most of his friends as "the Greek," is a former resident of the fair city of Hamilton. In his junior year he displayed qualities which resulted in his appointment as C/W/C in his senior year. He held this position both in the first and last terms.

Mike, a strong sports enthusiast, represented the college in his junior year in football, volleyball and boxing. This year he was a mainstay in the scrum of the College rugby team as well as being a member of the water polo team.

Mike always thinks things over carefully before making plans and when he acts he invariably does the right thing. Being a hard one to keep down, he is sure to be successful in his following years at R.M.C. The best of luck, Mike.

**DAVID ALAN THOMSON—No. 5302**

Weston, Ontario
Army

A former regimental signaler of the Queen's York Rangers, "Davy" is continuing his career in supporting the "Queen of Battle" as a tanker. A member of the head table for the entire year, Dave's success can be attributed to his resolute keenness and interest in the services. He has held the top three appointments in the college and at the same time made history by being the first CWC to be baptized in the middle lake by the senior term after having been baited by a beautiful red-haired young lady of Victoria College. He increased his prestige by being top cadet in the ROTP troop during summer training. Davy has done a lot for the college through his work as the "Log" business administrator. This ardent member of the rep billiard team is always heard quoting Shakespeare—"To pit or not to pit."

**ALBERT WILLIAM PCHAJEK—No. 5284**

Winnipeg, Manitoba
Air Force

Al, a graduate of Miles Macdonell Collegiate in Winnipeg, arrived at Royal Roads in the autumn of 1957. He comes from a family of seven brothers and three sisters who must have kept him hopping in his early years, for he broke the Manitoba record for the mile run while in high school. In his junior year at Royal Roads he was awarded the Director of Studies' Cup. Al has been outstanding in cross-country running and boxing, and has been one of the driving forces in LaSalle's sports triumphs. His favorite hobbies are photography and model aircraft.

As a first term flight leader, Al became known as Mr. "Pay-Check," a title he kept, for he was Number Three Squadron Leader for the second and honour terms. Although some of the less informed of the term think the Air Force is becoming the "Tiffany Corps," Al still has his sights locked onto being a top-notch pilot for the R.C.A.F.

**JON CLARK PELLOW—No. 5285**

Ingersoll, Ontario
Army

To the members of the Senior Term of 1958-59, the name Jon Pellow is synonymous with activity and enthusiasm. During his second year at Royal Roads he was appointed Cadet Squadron Leader for the first and third terms and captain of the rugby team. He was also a member of the representative volleyball and swimming teams and he proved himself an ardent hockey player and accomplished boxer. Along with his full-time task as Squadron Leader, "Juan" is responsible for the supply of Ingersoll Cheese to his term-mates, and he is always available to extol the virtues of his home town product.

Following his graduation from R.M.C. with a degree in General Science, Jon hopes for a regimental posting to the P.P.C.L.I. with a tour of duty in Germany. Eventually he holds high hopes of settling down as a physical training supervisor in the area surrounding his home town of Ingersoll, Ontario.

**RALPH LEONARD SYKES—No. 5297**

Calgary, Alberta
Navy

This Red-Headed Yahoo from Calgary with his sense of humour, tact and wit has won for himself throughout the college a reputation as "Cadet Wing Nice Guy." In both his years at Roads, "Good Old Ralph" has been the life-and-soul of many parties. As a cadet officer, he has never been without a date and has missed none of the many dances and parties to which he has been invited.

Ralph has been very successful at Royal Roads. As captain of the representative soccer team, as Number One Squadron Adjutant first term and as Number One Squadron Leader second and third terms, he has led by example and has gained the confidence and co-operation of all the cadets.

Calgary born and bred, "R.L." is a graduate of Crescent Heights High School. He intends to take a General Science course at R.M.C. next year and upon graduation to make a career of flying or navigating for the R.C.N.

**LAWRENCE ALEXANDER GIBBON—No. 5250**

Port Arthur, Ontario
Air Force

Larry, a graduate of Port Arthur Collegiate Institute, has been a cheerful companion throughout our term's two years at Royal Roads. A member of Fraser Flight, his various appointments include Deputy Cadet Wing Commander first term, Leading Cadet second term, and Cadet Squadron Adjutant last term. Larry is a proud radio officer of the Air Crew list, Observer branch. He excels in the field of radio transmission and is a licensed amateur radio-operator, call sign VE3EGJ. In any of his spare time he can be found contacting distant operators, but most often his own folks back home. Larry is gifted academically and was awarded the coveted Governor-General's Bronze Medal for attaining the highest marks in the Junior Term. This efficient, brilliant, hard working cadet is planning on Engineering at R.M.C., and will certainly attain his goal and make a fine Air Force Officer.

**KENNETH JAMES PARKES—No. 5283**

Cobourg, Ontario
Air Force

Ken is a cadet of many varied experiences. Born in Ottawa, he has moved many times from Ottawa to Kingston with a five year interim at a small town called Calgary.

Because of his exceptional ability with a trumpet, upon which he capitalized as an orchestra leader in Ottawa and Cobourg, Ken was a natural for the band. The first cadet to complete his first year at Royal Roads without losing his civilian status, Ken was the recipient of the year's "Gold-Plated Excused Chit."

Ken has made quite a success of second year, as a first and third term Squadron Adjutant and second term D-C.W.C. He is also Vice-President of C.A.I. and a member of the Representative Rifle Team.

In view of his previous accomplishments, his success in the next two years at R.M.C. and a third at Queen's is assured, but his future success with a certain red-haired "Western Belle" is uncertain.

**MICHAEL FRANCIS PETER—No. 5286**

Regina, Saskatchewan
Air Force

One of the more successful cadets from Saskatchewan, "Mike," via Camplon College, has emerged as a Cadet Squadron Adjutant in the honour slate. A Leading Cadet first term, his quiet personality, efficiency and loyalty gained the rank of Cadet Squadron Adjutant for him in the second term. He was retained in this position for the honour slate.

Modest about his abilities in general, he is particularly self-effacing regarding his athletic prowess. A dazzling surprise in hockey and a greater surprise as scrum-half on the rugby field, he has contributed much to both flight and college sports. His ability was further demonstrated by his appointment as captain of the representative volleyball team.

He plans to take a General Science course in preparation for his career in telecommunications, but his ability as an amateur photographer is not to be overlooked. With his humorous and unusual remarks, Mike has been a welcome addition to the College.

**DONALD JAMES ANDERSON—No. 5226**

Camrose, Alberta
Air Force

Don, or "Andy," is one of those Observer types in the R.C.A.F. He is one of the original prairie boys with his wild rose back home and none to match her. He hails from Camrose, Alberta, and is a staunch supporter of anything western. His eagle eye has been employed with excellent results on the rifle team.

In his first year Andy spent most of his time arguing with his brother, and using all of his resources to further his Flight. Andy showed his pugilistic abilities in his first year and finished by fighting his brother Lloyd for the championship. After spending most of his first year trying to decide whether to leave or stay, Don decided on the latter and went on in his senior year to be appointed Cadet Flight Leader of Hudson Flight for the first and third terms. With a gruff voice and undaunted determination to achieve nothing but the best, Andy led Hudson Flight to a very successful year.

**PHILIP GILBERT MARVIN ANTONSEN—No. 5228**

Sioux Lookout, Ontario
Navy

"P.G.," arriving at Royal Roads from the thriving community of Sioux Lookout in the wild regions of Northern Ontario, soon showed his value to the College in football and many other sports and activities.

In the gunroom as well as the sports field and classroom, Phil excels, for he either joins or starts one of the "friendly" discussions which hold the College together. Being a staunch naval man, he is invaluable in building up the spirit among the minority navy group. His achievements in the classroom and in military activities account for his place among the Cadet Officers in the first and last slates as a Cadet Flight Leader. Phil is also one of the famed "Gruesome Twosome," who are the terror of the Junior Term.

His plans include R.M.C., then command of his own ship, and, finally command of the whole navy.

**ROBERT WEBSTER CRAIG—No. 5237**

Calgary, Alberta
Army

C-F/L R. W. Craig, more commonly called Bob, has had a very enviable career at Royal Roads with regards to sports, academics, leadership and social activities. His determination, drive and athletic ability have been very evident in rugby, boxing and gymnastics, and he has always placed near the top of his term in academics. A Senior in Hudson Flight during the first term, he received his promotion and proved himself a very able Flight Leader during the second and third terms. His personal activities in Victoria cannot be disclosed at this point. It is quite certain that Bob will do very well in Electrical Engineering at R.M.C.

**EDWARD EXLEY—No. 5248**

Cornwall, Ontario
Army

Born in England, "Easy Ed" moved to Canada and took up residence in Ontario. He attended St. Lawrence High School in Cornwall and graduated in 1957.

Throughout his first year at Royal Roads, Ed was a staunch member of Fraser Flight. His outstanding ability in soccer earned him a position on the Rep Soccer Team. In his second year Ed became co-captain and was one of the major driving forces on the team.

As Cadet Wing Adjutant for the first term of his second year, "Easy Ed and his circle-chit-six" managed the Cadet Office in an efficient manner. His efforts gained him the dizzying heights of Cadet Squadron Leader in the second term. For the final term he was selected Champlain Flight Leader.

Ed hopes to join the Lord Strathcona's after an as yet undecided course at R.M.C.

**WAYNE STARR HAMMOND—No. 5260**

Pilot Mound, Manitoba
Army

Wayne came to Royal Roads from Pilot Mound. His drive and determination last year earned him Cadet Flight Leader I/C Band in both the first and last terms.

In both his Junior and Senior years Wayne has put spark into LaSalle Flight's drive to the top in sports. He has also contributed to the Rep Rugger and Volleyball Teams.

After spending last summer at Camp Borden, Wayne decided to switch to the Artillery and missiles. Wayne has still managed to remain in the top ten of his term even though he never misses a weekend with his girl in Victoria.

Wayne leaves Royal Roads for R.M.C. and a course in Mathematics. His quiet but pleasant manner is admired by all his term, and if he maintains his record, he is destined for a bright future.

**ARTHUR DENNIS HOPKINS—No. 4916**

Lacombe, Alberta
Army

Dennis, known to us as "Hoppy," is eager to finish a Commerce and Finance course at R.M.C., then get into his "turret." He has climbed to C-S/A and C-F/L in his second and final term this year after seeing action as Leading Cadet and C.W.W.O. last year. In addition to these deserving accomplishments, "Hoppy" has brought us successfully half way through our Senior Year as Gunroom President as well as being Sports Editor for the Log.

Dennis, a cadet of many talents, is the "Armoured Corps" at Roads. When not "traversing right or left" he is beating a favourite tune with the nearest two drumsticks.

When "Hop" can't be found, one might try his second home, the Gymnasium, where he practices his "swishes" that make him an outstanding player this year as Captain of the "R.R." Basketball team.

**RICHARD WILLIAM KUNTZ—No. 5270**

London, Ontario
Air Force

If you happen to be in the Gym at Roads and see a handsome individual bounding over the box horse or pushing up bar-bells, you will probably be seeing Dick Kuntz.

Dick hailed from London South Collegiate, London, Ontario, and adjusted to the rigors of Junior year with ease; so much so that he entered his Senior year with high academic standing and One Squadron's Four Bar position. Dick's exceptional drive and leadership have returned him to C-F/L of Fraser Flight for the honour slate.

Dick's winning smile has made many friends in Victoria, particularly with the weaker sex, though his primary interest is a certain nurse in London.

After emerging from the sands of Camp Borden, Dick decided to transfer to the R.C.A.F.

With such a background, Dick is sure of success in Civil Engineering next year at R.M.C.

**DAVID ROBERT GOWDY—No. 5253**

Galt, Ontario
Army

"Old Man Gowdy from Galt" came to Royal Roads to start his career in the Canadian Army. A former Militia Second Lieutenant in the Highland Light Infantry of Canada, he is very keen on a career in the infantry and hopes to become a Technical Staff Officer.

A strong supporter of Fraser Flight, he has been active in all flight sports. Fraser Flight's only Leading Cadet first term, Dave became Cadet Squadron Adjutant second term and Cadet Wing Warrant Officer last term.

In his first year summer training, Dave did very well and became Company Sergeant-Major for his graduation parade at R.C.S. of I.

Dave's two main interests are collecting antique fire-arms and enjoying classical and semi-classical recorded music. He plans to take Mechanical Engineering as the next step to becoming an infantry officer.

**JOHN HENRY BERNARD—No. 5232**

Edmonton, Alberta
Air Force

"Mabel" was born in Regina, Saskatchewan, in January, 1940. After moving to Edmonton, he graduated from St. Joseph's High in 1957. John plans to continue his academic training in Engineering Physics at R.M.C.

He received a flying scholarship from the R.C.A.C. With this to stimulate his interest in flying, he decided he wanted to become a pilot in the R.C.A.F. This he did and is now a member of the infamous term of '57 - '59.

John's interests are quite varied. He likes music, dancing, model building and skating. His interest in music was typified by his appointment as Cadet Band Master second term, and his volunteering to instruct the bell ringers for the Christmas Carol Service.

**BYRON DOUGLAS CLARK—No. 5234**

Melfort, Saskatchewan
Air Force

Before entering the hallowed gates of Royal Roads, "By" had distinguished himself as President of the Students' Council back home at Melfort Composite Collegiate Institute, and as an A-1 baseball player for the Melfort Juniors, who won the Saskatchewan championship from 1949 through 1953. He brought this athletic ability to Roads, where he centered for the "power line" of Number 3 Squadron's hockey team.

Being L/C first and last terms and C-F/L the second, Byron contributed considerably to the standard of Hudson Flight drill on parade.

His ambitions at present appear to be well considered. With the intention of obtaining a major in English at R.M.C., and becoming an official "fixer-upper" in the Air Force after graduation, Byron is certain to succeed.

**GEORGE WALDEN DALLEY—No. 5240**

Richmond Hill, Ontario
Air Force

George, a true blue Easterner and Toronto fan, graduated from Richmond Hill High School and has every intention of graduating from R.M.C. in Electrical Engineering. Not being a book-worm at heart, he still manages to get good marks.

A faithful member of excused parade, George has displayed his marvellous ability to "scull out" to a great extent this year, but he did take time out to hold the position of Flight Leader in the second term.

His favourite sports include football, hockey and one which requires a great many stamps and envelopes. "Duty-cadet, where's the mail?"

George has spent one summer with the R.C.A.F. in Winnipeg and hopes some day to become a back-seat driver in a CF-100.

**GRAHAM JOHN DALLIMORE—No. 5241**

Perth, Ontario
Army

Graham entered these hallowed halls of learning with his sights set on a degree in mechanical engineering and a commission in the Army. He is one of the elite group of cadets known as "those of the armoured corps." After a successful junior year, he descended on Camp Borden and spent his weekends commuting between there and Perth on his motorcycle. He returned to the college as a first term Cadet Flight Leader and carried out his duties well, eventually ending up as a Leading Cadet in the final term. Besides his academic ability and enthusiastic support of Fraser Flight on and off the sports field, he has a very active interest in Victoria. We know he will continue to do well at R.M.C.

**ANTHONY TALBOT DOWNS—No. 5244**

Calgary, Alberta
Air Force

Nineteen years ago, in Surrey, England, the Downs family welcomed Anthony Talbot into the fold. After nine years in Surrey, Anthony moved with his family to Vancouver, and subsequently to Calgary. "Tony" returned to the B.C. coast in 1957 with a telegram from the D.N.D., and high hopes of a future in the R.C.A.F.

Our "gangrene kid" found it difficult to adjust to the enormous responsibilities of Senior and Leading Cadet, but finished his career at Royal Roads as Librarian. Among his activities, "T-D" lists fencing, chess, sailing and riding. He likes music, loud bathrobes and Zorro, but abhors bandmasters and ham. His present ambition is to cross swords, C.S.C. pattern, with a certain member of the drill staff.

**DONALD EDWARD FALLON—No. 5249****Camp Wainwright, Alberta****Army**

After graduating from Wainwright High School, Don came to Roads. His first year was filled with many trials and tribulations but he soon became a cadet well worthy of the name.

In his second year, Don's leadership ability shone forth and he became a leading cadet for the last two terms. "Corrective criticism" his motto, Don straightened out many a junior on life at C.S.C. Royal Roads.

In the sports department, Don took a deep interest in LaSalle's interflight competitions for one and three-quarter years. From then on it was Hudson! Hudson! Hudson! Don still found time to captain the representative water polo team and lead them to several victories.

On graduation from Royal Roads, Don will seek an Engineering degree at R.M.C., and from there it will be back to the tanks.

**DAVID THOMAS LIGHTBURN—No. 5272****North Vancouver, B.C.****Army**

David Thomas Lightburn comes to us from Cobourg, Ontario, and now lives in Vancouver, British Columbia. Originally, David hailed from England, arriving in Canada in 1953.

An ardent supporter of MacKenzie Flight, he has done more than his share to uphold its traditions and promote its successes. In College activities, Dave is well known for his ability in soccer. In his Senior year he played goalie on the representative team and is well known for his many shut-outs.

On the military side, Dave showed his abilities and was made Leading Cadet for the second and last terms.

An above average student, Dave intends to continue his studies at R.M.C. in Civil Engineering. With his jovial nature and determination, we are sure Dave will make a success of his planned career in the R.C.A.C.

**EARL ROBERT LYTLE—No. 5274****Sharbot Lake, Ontario****Army**

Earl is one of the few cadets to make the grade from the battle-scarred ranks of Thompson Flight (Andrew's Rangers). He was transferred to Fraser in his second year and earned the rank of Leading Cadet in the second term. Now, in the third term, he has kept the rank but has changed homes, moving to his sister flight, Cartier.

Earl is a product of the Sharbot Lake High School of Sharbot Lake, Ontario. Either soccer is the main sport in his hometown or else Earl is just a natural, for he is an outstanding soccer player and consequently has earned a berth on the representative soccer team. He is also an avid hockey player and boxer of uncanny ability, being one of the most stylish counter-punchers in the College.

There is never a dull moment when "Lytle" is around, for he is full of that quick wit that tends to make him an outstanding character. He hasn't yet decided on a course at R.M.C.

**JOHN ROBERT (DIGGER) MACDOUGALL—No. 5276****New Toronto, Ontario****Army**

Military life is not the newest thing for Digger since he is an old hand at the Signaller's trade. After six years in cadets and three years in the militia, Bob had no trouble in obtaining his golden gaiters after only three months of life at Roads.

Bob has chosen the armoured corps for his career so that he can add his signal proficiency to the "Punch that paralyzes the enemy."

As a leading cadet for the first and last term and a flight leader for the second term, Bob did a great deal towards pushing Mackenzie Flight ahead in sports and drill. Digger is often seen at the various parties around town with a certain red-haired damsel. Bob also produced a fine showing on the representative soccer and rifle teams, going on the trip to Colorado in November.

With the drive and leadership that he has shown at the College, Bob will have little trouble in obtaining success in his chosen career.

**DAVID CHARLES SMITH—No. 5293**

Calgary, Alberta
Army

As the only surviving member of Champlain Flight, "Smithy" has done quite well in his senior year at Roads. He raised the Flight to distinction as C-F/L in the second term, and in the final term became a distinguished member of the "one-bar-wonder club." Born and raised in Calgary, Dave was educated at Calgary Central High School, where he took an active interest in sports. He represented Roads in football and cross-country as a junior and in rugger, cross-country, basketball and water polo in his senior year.

His plans for the future lie in R.M.C. and, most of all, a career in the P.P.C.L.I.

**VICTOR ROY THOMAS—No. 5301**

Fort Saskatchewan, Alberta
Air Force

"Vic" Thomas, another staunch member of the famed "Goofers" hails from Fort Saskatchewan, Alberta. This year he has proved to be a valuable member of Champlain Flight and an indispensable asset to the Royal Roads representative rugger and boxing teams. A rugged, hard-driving player of the "second row" on the rugger squad, his aggressiveness also led him to cop the heavyweight championship of the College. Vic is good in academics as well, for he is tops in all subjects and is constantly being sought after by the senior term when chemistry problems are assigned.

First year of summer training saw "Vic" screaming through the skies over Centralia in his "Chippie," maintaining the standard of high-flying "B" flight. "Vic" has proved to be an all-round cadet, being Leading Cadet for the last two terms and excelling in sports and academics. His favourite time of year seems to be June in any season.

**HERBERT RAYMOND WIDDIFIELD—No. 5307**

Bienfait, Saskatchewan
Army

Herb ventured forth from Bienfait, Saskatchewan, in the fall of '57, crossed the threshold of Royal Roads and was quickly introduced to a new manner of existence.

Herb applied himself from the beginning and played a major part in La Salle Flight's drive for sports supremacy. He also found time to represent the College in cross-country running and rugger. Another of his accomplishments was the position of Leading Cadet in the second and third terms, in which capacity he put forth a maximum of effort.

Saskatchewan gave to Royal Roads a cadet with an abundance of wit and a sense of humour. Any member of the senior gunroom will testify that many of the skylarks taking place there could be directly traced to this red-head.

At R.M.C. Herb plans to take Electrical Engineering, and with his past academic record he should have little trouble.

**JOHN MICHELL BUCHAN—No. 5233**

Toronto, Ontario
Army

John is a native of Ontario and he, being of hardy Eastern stock, was chosen as a first term S/Adj.

Aside from the military side of College life and the "pit," our Artsman friend does wonders with a squash racquet. He has also defended the goal of the 3 Squadron hockey team for two years. Besides these accomplishments, John is well known as an ardent skier.

As one of the last of the Infanteers, his future seems to be wrapped up in the Black Watch plaid.

**WHITNEY ARTHUR COOK—No. 5236**

Collingwood, Ontario
Navy

Whitney came to R.R. from the shipbuilding port of Collingwood, Ont. In his Junior year, Cookie played in the band, was a member of the Cross Country team and upheld Hudson Flight. It is also rumored that he is an ardent cow roper and "pomme" getter.

During the summer, Ted was in his glory when on the bridge of the "O" boat. It is understood that his Naval Reform policy along Great Lakes sailing methods is up for second reading.

This year Whitney has settled down to studying. When not studying he can be found playing squash, at which he is one of the top players on the representative team.

After R.M.C. and a degree in Mechanical Engineering, he plans to go into Naval Construction. We are sure that his infinite determination will make him successful in his chosen career.

**RICHARD HOOPER CRANE—No. 5238**

Toronto, Ontario
Navy

Another of the somewhat outnumbered Naval Cadets at R.R., Dick—more commonly known as Hooper to his many friends—originally hails from Rugby, England. He now makes his home in Toronto. Dick has spent a very successful two years at R.R. and will be remembered by his classmates as the Master of the Bright and Comical Side of Life. His skill with books paid off with his appointment as Librarian first and third terms. Among his activities at the College are the Debating Club, Member of the Advertising Committee of the Log, and Senior Gunroom Treasurer.

Dick plans to study Electrical Engineering at R.M.C. next year, and with his wonderful sense of humour and interest in the sea, Dick is almost certain to make a success of his Naval career.

**JAMES STUART ELDER—No. 5247**

Ottawa, Ontario
Army

Stuart, a product of the infamous Ottawa Valley, comes to these fair halls of learning from Glebe Collegiate Institute. This easy-going amiable type, with his keen sense of humour, has certainly been no disappointment to the R.O.T.P.

In his junior year, after running too many circles because of rigorous morning inspections, "Stu" donned the cougar skin and the big bass drum. For his efforts in first year "Stu" returned as a Leading Cadet.

In sports, a high competitive spirit and ability has rewarded him with success in inter-flight matches, as well as a position on the representative football and volleyball teams.

As a conscientious artsman, "Stu" plans on majoring in history at R.M.C., after which he will accomplish his ultimate goal as an Infanteer.

**MARCEL JULIEN JOSEPH GILBERT—No. 5251**

Longlac, Ontario
Air Force

Longlac, Ontario, is well known to the senior term as the home town of Marcel. He obtained his Senior Matriculation at Geraldton High School before coming west for the best years of his life at Roads.

Marcel is tops in academics and distinguishes himself in sports as one of the best hockey players in the College. Many a game has been won by his drive and enthusiasm.

A member of Fraser Flight in his junior year, he was transferred to Cartier Flight and was Cadet Librarian in the first term of his senior year.

His sense of humour has been known and appreciated since his arrival at the College by the numerous skylarks in which he took part.

Summer training takes him to Winnipeg where he is presently learning the art of navigation. Marcel will carry on to R.M.C., where he will continue his studies in Civil Engineering.

**EARL EUGENE GOSKI—No. 5252**

Froude, Saskatchewan
Air Force

Earl was born in Southern Saskatchewan, Sec. 25, Township 9, Range 11, west of the second meridian, and he has credentials to prove it. Graduating from Froude High School, Earl came to Royal Roads for a rest. After a brief and puzzling junior year, he emerged a senior—Leading Cadet in the second term and Librarian in the honor slate.

In sports Early showed his athletic ability in water polo, rugger, swimming and curling.

In the years to come Earl will be well remembered for his super deluxe triple coat hanger radio antenna, which could be seen hanging from his window every day (except Tuesday, of course). No doubt the Air Force will find the same contraption flying gaily through the skies with Earl as he learns the tricks of navigation at Winnipeg during summer training.

**GWYNFRED ALFRED GRIFFITH—No. 5256**

Dawson Creek, B.C.

Air Force

Well known to us as "Griff," Gwynfred's time is spent in and out of liberty boats and Sick Bay. Sometimes he is observed taking time out for studies.

Griff has been very athletic and has participated in rugby, swimming, volleyball and boxing. Although he was downed several times on the sports field, Griff has managed to regain his stand and fight back.

In the gunrooms, if one listens to the general conversation, you are likely to hear Griff shouting for Dawson Creek, B.C. Griff is one of the few cadets from B.C. in the class of '59.

Good luck at R.M.C. and in the R.C.A.F., Griff.

**MERLE JOHN GROTT—No. 5257**

Hanna, Alberta

Army

From out of the "Big Country" of oil rigs and wheat fields came the only R.C.E. man of the senior term, Merle Grott. Although the target of many appeals that he change corps, his sense of humour and determination soon convince his opponents that Chilliwack is "heaven on earth."

In his first year Merle proved himself to be a strong supporter of La Salle Flight but he was moved to Mackenzie Flight as a Leading Cadet first term for his second year.

Merle is an excellent athlete and proved it as a guard on the representative football team the first year and as an outstanding cross-country runner in his second year. His ability and enthusiasm for hockey led his squadron to second place in the inter-squadron league.

When he graduates in 1961, R.C.E. will be better off by having obtained another outstanding officer in Merle Grott.

**DONALD ALLAN HARRIS—No. 5261**

Cobourg, Ontario

Army

Don came to Royal Roads as one of the youngest cadets to enter the College. No stranger to military life, he had served with the 33rd Medium Artillery Regiment. He took his first summer at Camp Borden with the Infantry. Keen and hardworking, he was always "alpha-plus" in turn out, and was one of the best trench diggers in the company.

Don is one of our few artists, but this affliction does not stop him from being one of the top students in the College. Besides giving his time to "The Log" and flight activities, he still manages to keep ahead of his assignments. He manages infrequently to write to a certain Diane back in Cobourg.

Don has a very likeable personality and gets along with everyone. With his drive and determination, he will do well at R.M.C., where he plans to take the Commerce course.

**ROBERT GORDON HAWKINS—No. 5263**

Toronto, Ontario

Navy

If you ever happen to call one of the Navy's ships a "boat," you had better be certain that you are far out of the hearing range of "Gordie" or you will be severely reprimanded. A staunch supporter of the Navy and one of the many cadets who hail from TO, Gord graduated from Etobicoke Collegiate in 1957.

One of the few cadets who was not transferred out of "good old" Fraser Flight this year, Gord's nonchalance, utter confidence and excellent humour added much to making Fraser the unique flight that it was during our junior year. "Our Little Admiral" has left many memorable events behind him, such as that fateful Sunday during parade, and the tumbling practices that he so gladly provided his mattress for.

Next year Gord plans to take the General Science course at R.M.C., with an eye to a future in the R.C.N.

**NORMAN INGLIS—No. 5266****Abbotsford, B.C.****Navy**

"Norm," a native of Abbotsford, B.C., has spent his two years at Royal Roads keeping up the tradition of Fraser Flight. Always a hard worker, his keenness paid off when he was chosen Librarian for the second term.

Norm is a good competitor in sports and athletics, especially in soccer and basketball, in which he played on the representative teams. His hobbies are confined mainly to sailing and to rolling his own cigarettes, in both of which he is very skillful.

The Fleet Air Arm is his ambition, and if his first year of summer training is any indication of his success in the R.C.N., he should do quite well. Norm was appointed Cadet Captain of White Watch during our third two-week period at sea and finished the summer with a good record.

With his continual cheerfulness and sense of humour, he is bound to make a success out of his Naval career.

**RUDOLPH J. KOVACS—No. 5268****Vanessa, Ontario****Air Force**

Two years ago Rudy decided to give up the easy-going life of a civilian and join the Service. He arrived at Royal Roads on that memorable day and soon settled down to his work.

Rudy was a first-class junior cadet and consequently became a Leading Cadet in his first term as a senior. He is a staunch supporter of both Cartier Flight and the R.C.A.F.

"R.J." has developed into a fine boxer and was a member of the Royal Roads' boxing team that visited Kingston this year. He takes up weight-lifting as a hobby and is the "J. Arthur Rank" of the senior term with his movie camera and projector.

After he completes his course at R.M.C., Rudy will become an officer in the R.C.A.F.

**JOSEPH ALEXANDRE MARC LANDRY—No. 5271****Ottawa, Ontario****Air Force**

Marc, better known as "Jam" to his friends, proceeded from Ottawa University to Royal Roads in September, 1957. His junior year was a success although he was a member of the "Big 12," and constantly required supports for chronically weak ankles. A Leading Cadet in the second term, he also stood better than average academically. Training as an observer in the R.C.A.F., Marc spends his summers in Winnipeg.

It is rumored that Marc's latest ambition is to write a revised version (probablement en français) of "Tom Jones' Cabin." Everyone understands that this ambition stems from his admiration and friendship for his favorite sergeant at Roads.

Marc's planning on acquiring a Civil Engineering degree after graduating from R.M.C. Marc's keen sense of responsibility and sense of humour will make him as popular and successful in the future as he has been during his two years at Royal Roads.

**SIDNEY ALAN MONEY—No. 5280****Lindsay, Ontario****Air Force**

Syd comes from Barrie, Ontario, but frequently visits Lindsay when on leave. Well known for his distortion of the English language, Syd is popular with his termmates.

A faithful two-squadron member for two years, he has shown his ability by being Mackenzie Flight Leader the first term.

Syd spends his summers at Camp Borden and Clinton loading aircraft with rockets. Syd is very helpful, and if a question is asked of him, he immediately comes out with his famous answer, "so."

A well-known College sportsman, Syd has represented Roads on both football and boxing teams. His favourite sport, however, comes between the hours of 2230 and 0620.

Syd plans to take Electrical Engineering next year at R.M.C. Despite all the above truthful statements we hope Syd has a bright future. Good luck, Syd.

**ROBERT WILLIAM MUNDAY—No. 5281**

Oshawa, Ontario
Navy

Our Oshawa chap is fairly quiet and reserved but set in his ways. A member of Champlain last year, Bob was moved with most of his flight this year to try and strengthen the junior half of No. 2 Squadron.

In his Junior year, Bob was always referred to as "the Arts student," and he has made that good this year by joining the "select six." Along this line, he was editor of "The Twig" and received nothing but compliments for his work. Also he was recognized as an expert of another form, being chosen Librarian for two terms.

He is an experienced curler, and skipped a rink in his Senior year. This year he also took up the art of toad-stabbing (fencing).

Naturally "Cadet Munday" is looking forward to R.M.C., when he will be just a short jaunt from home.

**ALBERT ERNEST PARNETTA—No. 4932**

Canora, Saskatchewan
Navy

Al, already a member of the Ex-Cadet Club, consented to return to Royal Roads this year to give the soccer team a hard-driving fullback. Unfortunately, his spirited play led to a broken arm, which cost him the chance to play against the U.S.A.F. Normally, "Parnelli" can be found glued to a chair eight feet from the TV screen or else describing the terrible injustice of his latest charge. When Fraser Flight is on duty, Al does his best to keep the library running in its usual state of mass confusion. A strong Navy man, Al is forever proclaiming the wonders of H.M.C.S. "Margaree," in which he sailed last summer. The "Navy Corner" of the mess attracts his full attention, and there, as usual, the "Senior Service" reigns supreme.

**DAVID BOLAND PORTER—No. 5287**

Fort William, Ontario
Air Force

Probably the only French Orangeman to attend Royal Roads in recent years, Dave is well known to his termmates as a most cheerful, sincere and dependable friend. His interests focus primarily on volleyball and water polo, academic endeavours and, finally, arguing. Any time of the day or night is quite suitable to Dave to open a discussion on such things as the advantages of speaking with an Ottawa Valley accent.

During the next two years, David will be majoring in History at R.M.C. and, following graduation, looks forward to frequent long, quiet trips over the Pacific as a radio officer at Comox. His plans for a career in the R.C.A.F. are not definite, as civilian employment appears to hold considerable fascination for him.

**STANLEY CHARLES SHEPHERD—No. 5291**

Elgin Falls, Ontario
Air Force

Stan was born and raised in Elgin Mills, Ontario. He took his high schooling at neighbouring Richmond Hill, where he must have developed his interest in airplanes and flying. Needless to say, he is a pilot.

Stan spent his first year in Champlain Flight and his second in Mackenzie. Throughout he has been a staunch supporter of all activities, with special interest in volleyball and squash. In his spare time Stan is occupied with his camera or, judging from the strange variety of sounds issuing from his cabin, his records.

Stan is an excellent student, and following graduation from R.M.C. he anticipates further studies in Engineering Physics and Aeronautical Engineering.



MICHAEL BRYCE SULLIVAN—No. 5295

Montreal, Quebec
Army

"Mike," or "Sully" as he is better known, is a native of Montreal, but coming from a military family, he had the opportunity to move about Eastern Canada.

True to his "fighting Irish" name, Mike has chosen the army and in particular the corps of corps, the R.C.A., as his future. In this regard he hopes some day to be chosen as a Technical Staff Officer with this corps.

In both his junior and senior years here at Roads, Mike has taken an active part in many College activities and has held positions on both the Representative Cross-Country and Rifle teams. He was also a Cadet-Flight Leader during the first term.

A good student and hard worker, Mike's future, as a cadet at R.M.C. and later as an officer, is assured. Moreover, it is well known that Mike can hardly wait to see "R.M.C."



JOHN DONALD SUTHERLAND—No. 5296

Ottawa, Ontario
Navy

"Suds" is well known for his ability extolling the virtues of the Ottawa Valley, or more so, Hull, Quebec. (What have they got in Hull that is not in Ottawa, Suds?)

His inept ability to "panic the wing" plus being a master at the fine art of pitting in class with his eyes open have been Suds' trademarks for the past two years.

Our ski-enthusiast also found time to play representative rugger and to support nobly dear old Champlain Flight in soccer.

As for the future, our boy in navy blue plans to use the Royal Roads lagoon as home port for his submarine.



ROBERT HALL THOMAS—No. 5300

Hudson Heights, Quebec
Navy

"Tex" came to us from Hudson Heights, Quebec, to see what the navy and Royal Roads had to offer. Learning that the only thing you get for nothing at Royal Roads is a charge, Tex has applied himself academically and on the sports field. He has been on the College basketball and swim teams and does well in all sports, but his favourite interests are sailboats, sports cars and girls.

Tex seems to never have any trouble getting a date, probably due to his keen sense of humor and quick wit.

With his love of wine, women and song (it's not as bad as it sounds), Tex is bound to have a successful and interesting career in the navy.



CALVIN GLEN VARDY—No. 5303

Bancroft, Ontario
Army

Glen graduated from North Hastings High School in '57 and proceeded to Royal Roads that fall to start his training as an officer in the Canadian Army. From the start he has been a dedicated infantryman, having decided on the R.C.R. as his regiment. After a summer at Camp Borden he was even more convinced that the Army was the only life.

Glen was a member of Vancouver Flight in his Junior Year and was transferred to La Salle for the final year. In sports, he has fought hard for the flight, standing particularly well in the cross-country.

Saturday morning finds Glen either in the dark room or playing chess with Stu. After numerous games it is said that he has finally beaten his arguing partner.

Next year Earl, I mean Glen, plans on taking an Arts course at R.M.C.

GRADUATING CLASS 1959

ANDERSON, D. J.	Enderby, B.C.
ANTONSEN, P. G. M.	79 Ethel Street, Sioux Lookout, Ont.
BERNARD, J. H.	10811 - 112th Street, Edmonton, Alta.
BUCHAN, J. M.	72 Cheltenham Avenue, Toronto 12, Ont.
CAVANAGH, M. W.	12 Westbourne Road, Hamilton, Ont.
CLARK, B. D.	Box 255, Melfort, Sask.
COOK, W. A.	570 Hurontario Street, Collingwood, Ont.
CRAIG, R. W.	#04 Windemere Road, Wildwood, Calgary, Alta.
CRANE, R. H.	41 Newton Drive, Willowdale, Ont.
DALLEY, G. W.	50 Loux Street, Richmond Hill, Ont.
DALLIMORE, G. J.	86 Drummond Street East, Perth, Ont.
DOWN, A. T.	427 - 33rd Avenue N.E., Calgary, Alta.
ELDER, J. S.	1560 Chomley Crescent, Ottawa 1, Ont.
EXLEY, E.	303 Shirley Street, Cornwall, Ont.
FALLON, D. E.	MPO 500, Camp Wain, Wainwright, Alta.
GIBBON, L. A.	198 North Algoma Street, Port Arthur, Ont.
GILBERT, J. M.	P.O. Box 204, Longlac, Ont.
GOSKI, E. E.	Fraude, Sask.
GOWDY, D. R.	45 Pollock Avenue, Galt, Ont.
GRIFFITH, G. A.	817 - 106 Avenue, Dawson Creek, B.C.
GROTT, M. J.	Hanna, Alta.
HAMMOND, W. S.	Box 303, Pilot Mound, Man.
HOPKINS, A. D.	5510 - 50th Avenue, Lacombe, Alta.
HARRIS, D. A.	Cold Springs, Ont.
HAWKINS, R. G.	381 Prince Edward Drive, Toronto 18, Ont.
INGLIS, N.	R.R. #1, Mount Lehman, B.C.
KOVACS, R. J.	R.R. #2, Vanessa, Ont.
KUNTZ, R. W.	79 Wharcliffe Road South, London, Ont.
LANDRY, J. A. M.	1900 Alta Vista Drive, Ottawa, Ont.
LIGHTBURN, D. T.	504 Saville Crescent, North Vancouver, B.C.
LYTLE, E. R.	Sharbot Lake, Ont.
MacDOUGALL, J. R.	172 - 5th Street, New Toronto, Ont.
MONEY, S. A.	20 Sunset Crescent, Lindsay, Ont.
MUNDAY, R. W.	710 Glencairn Street, Oshawa, Ont.
PARKES, K.	67 Abbot Blvd., Cobourg, Ont.
PARNETTA, A. E.	Box 567, Canora, Sask.
PCHAJEK, A. W.	Box 33, Group 320, R.R. #3, Winnipeg, Man.
PELLOW, J. C.	63 King Street West, Ingersoll, Ont.
PETER, M. F.	2065 Broder Street, Regina, Sask.
PORTER, D. B.	2230 McGregor Avenue, Fort William, Ont.
SHEPHERD, S. C.	Elgin Mills P.O., Ont.
SMITH, D. C.	616 - 13 Avenue S.W., Calgary, Alta.
SULLIVAN, M. B.	70 Nery Avenue, Picton Heights, Ont.
SUTHERLAND, J. D.	606 Denbury Avenue, Ottawa, Ont.
SYKES, R. L.	2640 - 1st Avenue N.W., Calgary, Alta.
THOMAS, R. H.	Hudson Heights, P.Q.
THOMAS, V. R.	R.R. #2, Fort Saskatchewan, Alta.
THOMSON, D. A.	9 Joseph Street, Weston, Ont.
VARDY, C. G.	Bancroft, Ont.
WIDDIFIELD, H. R.	Bienfait, Sask.

COLLEGE LIFE

COLLEGE CALENDAR

The following is a list of activities at Royal Roads for the Fall, Winter and Spring Terms, 1958-59:

- Sept. 3—Cadet Officers arrive.
 4—Visit of Officers and Cadets of Imperial Japanese Navy.
 New Entry Cadets arrive.
 18—Senior Term arrive.
 20—Recruit Interflight Tabloid Sports Meet.
 22—Classes begin.
- Oct. 8—Visit of Admiral Mountbatten, the First Sea Lord.
 11—Interflight Track and Field Meet.
 13-16—Passing Off the Square.
 14—Interflight Cross-Country.
 17—Obstacle Course.
 Dining-In Night.
 24-26—Military Weekend.
 31—Cadets' Hallowe'en Dance.
- Nov. 7-9—Stand Down Weekend.
 7-10—Visit to U.S.A.F. Academy.
 15—Hibbard Trophy Game.
 21—Visit of His Excellency the Governor-General.
 22—Invitation Cross-Country Race.
 28—Visit by Colonel J. W. Symons, Commandant R.C.S.A.
 Boxing Finals.
- Dec. 6-15—Christmas Examinations.
 14—Inspection of Cadet Wing by Commodore Groos.
 15—Christmas Dinner and Carol Service.
 16—Christmas Dance.
 Publication of "The Twig."
- Dec. 17-
- Jan. 4—Christmas Leave.
 5—Classes begin.
 6—Visit of Read-Admiral K. L. Dyer, Chief of Naval Personnel.
 10—Visit of Swedish Officers and Naval Cadets.
 15—Visit of Officers from 5 Air Division H.Q.
- 18—Visit of members of Victoria Branch R.M.C. Ex-cadet Club.
 23-25—Visit of High School Counsellors.
- Feb. 10—Invitational Swimming Meet.
 12—Inter-Squadron Hockey Finals.
 15—Inspection of Cadet Wing by Air Vice-Marshal Bryans, A.O.C. Training Command.
 18-19—Commandant's Conference at R.M.C.
 20-21—Conservcol Tournament at R.M.C.
 20-22—Stand Down Weekend.
- Mar. 9—Lecture on Soviet Armed Forces by W/C Kenny of D.A.I.
 15—Inspection of Cadet Wing by Rear-Admiral H. S. Rayner, Flag Officer Pacific Coast.
 20—Interflight Swimming Gala.
 Officer Cadets' Mess Dinner.
 23—Visit of Dr. Dooley, R.M.C.
- Apr. 2—Visit of Dr. W. L. Ford, Superintendent of the Pacific Naval Laboratory.
 9—Presentation of Rifle and Pistol Awards.
 11—College Regatta.
 12—Visit of members of Vancouver Branch R.M.C. Ex-cadet Club.
 15-27—Final Examinations.
 17-21—Visit of Brigadier R. P. Rothschild, D/ROTP.
 28—Visit of Air Vice-Marshal MacBrien, OBE, CD, AOC, Air Defence Command.
- April 30-
- May 6—Operation Northbound.
 8—Military Ball in Vancouver.
 10—Joint Parade with H.M.C.S. Venture.
 11-13—Supplemental Examinations.
 15—Senior Staff dine Graduating Class.
 18—Parents' Day.
 19—Graduation Ceremonies.
 Graduation Ball.
 20—Academic year ends.

INITIATION WEEK



By J/C M. W. Hewes

Initiation week at Royal Roads had always held an aura of mystery for me. I first heard about it through S/C Shepherd's parents over a year ago. They were showing me some pictures which they had of their son at college. They described the college in great detail (circles, charges, etc.) and showed me some pictures of the grounds and cadets. They then came to a small bundle of pictures at the bottom of the pile and that was where they stopped. Knowing that I hoped to go to Royal Roads they said that they would not show me these last few pictures, which were of initiation week.

Well, right there I began to wonder as to what I was trying to get into. For the next few months I tried to figure out what they would do to me if and when I got there. I made a point of not missing "West Point" on TV each week and tried to imagine my own fate from it.

During this time I was sent to London, where I went up before a selection board. I then chewed my nails for the next six weeks waiting for a telegram. It came and I was off to Royal Roads. In the course of the next few panic-stricken weeks the only thing that went into my panic-stricken head, and stayed there, was little snatches of conversation concerning "Initiation Week," particularly Trafalgar Day.

Here are some of the remarks heard: "There were more juniors in the hospital than at the college after initiation week last year"; "You may have to carry your buddies across the line to junior cadetship"; and "You should have seen S/C — last year at the mess dinner."

Well, by Sunday evening, October 12, ominous rumblings were heard. S/C Macdougall soon had his tank crews busy down in the Senior Gunroom corridor. Taking S/C Macdougall's lead, the seniors got in on the fun (fun for the seniors, that is). Come Monday morning and all hell had broken loose. All at once there seemed to be ten times as many seniors as there had been before; they were everywhere, doing everything to taunt recruits. The week passed quickly, little or nothing being learned in classes, and the seniors were just as unrelenting on Friday as they were that first Monday morning.

Friday afternoon was the climax to the whole affair and at about 1600 I became a junior cadet. That evening we had a mess dinner given in our honour followed by some entertainment on the quarterdeck.

All in all, we had a very exhausting week. Ask me if I'd like to do it again and I'll reply with an emphatic "NO." Ask me if I thought it was worth it and I'll answer with an emphatic "YES." After all, after getting to be a junior, the next step is becoming a senior.

U.S.A.F. ACADEMY SOCCER TRIP

By C/S/L R. L. Sykes

The exchange visits between the Royal Roads and the U.S.A.F. Academy soccer teams seem to be well on the way to being established as an annual event. This exchange consists of an annual soccer match battled out in Denver and here at Royal Roads alternately. It was for this event, then, that the "Sixteen Blue-Men" trained during the fall season, and in the company of two cadet spectators and a number of the senior staff, these same sixteen boarded an R.C.A.F. C-119 bound for the den of the U.S.A.F. "Tiger."

For most, the trip down was quite an experience. Someone affectionately referred to the C-119 as a "flying boxhorse" with the implication that he wasn't particularly enjoying the comfortable journey in the cozy stern of the marvellous craft. However, everyone busied himself with cribbage, reading or the ancient and honorable art of pitting. Soon we touched down at the U.S.A.F. base at Hill, Utah, and the first leg of the trip was over.

Everyone headed for the cafeteria in the "Ops" building and filled themselves with all the staple foods which constitute a cadet's diet—milkshakes, hamburgers and chocolate bars. We learned that a weather disturbance would delay us, so we made use of the TV set in the pilots' lounge and browsed through the post exchange.

Eventually we became airborne again and headed non-stop to Denver. Enroute, the going got a bit rough and we all depended too much on the airman on board to act like a T.C.A. hostess.

On arrival, we were greeted by members of the U.S.A.F.A. soccer team and were ushered into buses. Off to the Academy and eye-popping surprises. What a place! Everything is completely new. The buildings are of the most modern glass and steel construction, and their size is almost fantastic. One hall was so long that when Lytle knelt down and put his eye close to the floor, he swore that he could see the curvature of the earth! The dining hall would stand high in any comparison of eating places. Hundreds of waiters, good cooks and enjoyable background music filling the building made the meals more a pleasure than a necessity. To do this Academy justice, we could write all day, but we must move on to the game itself.

The soccer game was the next morning—Saturday, 8 November, 1958. We travelled

by bus to a stadium in Colorado Springs that doubled as a rodeo field, but which was quite protected from the brisk wind. The game was the toughest we had played all season, but the team all produced an extra bit of drive from somewhere that did them tremendous credit. The play was fast and the American squad proved to be excellent ball handlers. Constantly our goalie, Rocky Dave Lightburn, ruined the offensive drives of the opposition. At half time the game was a scoreless tie. Tension was high and both teams started the second half with vigorous ambition. Unfortunately, for some reason, perhaps the altitude of the plane trip, the Royal Roads team tired quickly and the game ended with a 3-0 victory for the U.S.A.F. team.

After the game, we returned to the academy, had a quick change and were off again in the buses for Denver City. Here we watched a football game between the U.S.A.F. Academy and Denver U. Then it was on to a party which alone was worth the trip. Dates were arranged for all of us and we, with the members of the U.S.A.F. soccer team, danced to the music of a little German band in an appropriately decorated hall. The evening passed in a frenzied round of fun and frolic with such spectacles as Cavanagh and Sykes attempting to follow Macdougallvitch's wild Russian dancing, and Stowe grabbing a trumpet and leading the band through certain musical interpretations hitherto unheard. Someone started the ball rolling, and soon a great number of us found ourselves escorting entirely different girls than our original dates.

As all good things must, the evening finally had to end. We all slept on the return trip to the Academy, but rose early the next morning and shuffled on board the C-119 for the return trip. We were again delayed at Hill A.F. base and were forced to change our route to an alternate through Great Falls and Lethbridge.

We arrived half a day late at the college and were welcomed back by the entire cadet wing. Without an exception, all of the soccer team members felt that this annual event is one that should and must be continued. In this respect we hope that the visitors next year will be treated with as much hospitality as we received from our neighbours to the south. With a fond look to the past, and a hopeful look to the future, we say "Vive les relations internationales." (The French is Mr. Sykes rather devious way of saying "Love those American girls"—Ed.)

GOVERNOR-GENERAL'S ADDRESS



His Excellency the Governor-General visited the College and inspected the cadet wing on November 21. After the cadet wing had advanced in review order, His Excellency addressed the following remarks to the cadets:

"I am delighted to be able to visit Royal Roads for the second time, on the occasion of my trip to British Columbia for the Centennial Celebrations. The form of the College may have changed since my last visit but the spirit, I know, has remained constant and unaltered. It continues to teach the traditional virtues of discipline, a sense of duty and good manners, virtues which are not universally respected in these days.

"I have greatly enjoyed, in the last few years, the privilege of visiting Service establishments. The civilian, through such visits, can learn what he should know about the character and problems of the three Services on which the defence of his country depends. He can also learn much of great educational value to himself.

"It is now nearly four hundred years since Sir Humphrey Gilbert urged Queen Elizabeth I to found an institute of military science:

"Whereas in the universities men study only scholastic learnings, in this Academy they shall study matters of action

meet for present practice both of peace and warre."

Since that time 'matters of action meet for present practice' have increased in number and complexity. Moreover the present is always vanishing into the outmoded past. It is no wonder that the officer is never fully trained, but is always in training. This must be so. As the price of military preparedness the machine must be continually renewed and its operators constantly re-trained.

" 'Nurture your minds,' said Disraeli, 'with great thoughts.'

"A modern philosopher insisted that education should include the 'contemplation of greatness'.

"Training can only add firmness and strength to a mind already made broad and generous by the study of literature and history. For it will have gained some acquaintance with the whole of Western civilization, the way of life cherished and defended by the Western world. Follow such a path and you will, no doubt, have familiarized yourself with your own country, its history, its traditions, its achievements and its shortcomings. Then will you become in knowledge, in conviction and in conduct the kind of person fit to show to others what our nation is, and why it must be defended."



Visit of Admiral of the Fleet, The Earl Mountbatten of Burma, 8 October, 1958



Christmas Ball

THE CHRISTMAS CAROL SERVICE



By S/C J. R. Macdougall

Once each year the college staff and cadets gather together for a quiet but meaningful carol service. The service was held again this year, after the Christmas dinner, in the main lobby of the castle, which was appropriately decorated for the occasion with fir and spruce boughs and tinsel. The cadets lined the stairs as in past years, while the staff sat before the crackling fire in the fireplace.

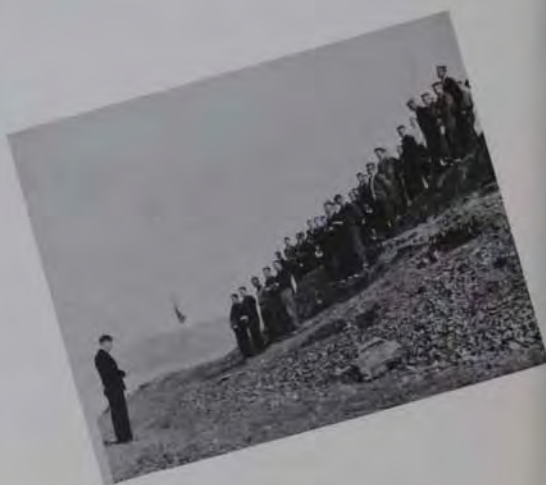
The two chaplains of the college, Padre Mowatt and Padre Belanger, led the service and together read the Christmas story. Traditional carols were sung by all throughout the service, accompanied by the organ played by Lt. Kadey. The choir, under Prof. Dutton,

added their rendition of the Christmas story with the singing of old English carols. The bell ringers, under L. C. Bernard, helped create a joyous Christmas atmosphere.

Before the service was over, the Commandant expressed his best wishes for the holiday season to the cadets and staff. He then introduced the Lieut.-Governor, the Honourable Frank M. Ross, who also addressed the cadets and guests.

After the service, the officer cadets were invited to the wardroom for refreshments. A number of officer cadets were introduced to the Lieut.-Governor in the conference room.

As this most enjoyable evening came to an end, all thoughts were directed to Christmas leave and the fast approaching New Year.



MEMOIRS OF OPERATION NORTHBOUND

By J/C D. G. Mills

One drizzly Wednesday afternoon in the last week of April, 1959, the P.A. system came to life and the cadet wing collectively pricked up its ear. An indistinct mumbling was heard, then a list of juniors' names was read off. By some remarkable act of fate some cadets had actually passed their final exams, and through the Cadet Wing's ability (developed through much experience) to translate the mumble of the "Old Man of the Castle" into meaning, it was discovered that this group (plus a few seniors who had passed and who didn't wish to go to the Military Ball in Vancouver (?)) would go camping for pleasure in the sunny north for a week. They labelled this pleasure cruise "Operation Northbound," which gave the suspicious air of what those in the Army call a routine field exercise. We disregarded this bad omen, however, and were immediately launched on an operation which seemed comparable to the North Pole expedition or, perhaps, the climbing of Mount Everest.

Within one-half hour of the pipe, we were issued a sleeping bag, eating utensils and one-half of a regulation Army pup-tent. (This, incidentally, was combined with another cadet's half to create one whole for two.) Instructions for "Northbound" were also handed out, including a copy of the daily routine. A few horrible statements caught my eye on looking through the orders. Such things as: Reveille, 0700 hrs.; discipline to be maintained at Royal Roads standard; a cross-country march from Nose Point to Ganges, and others had a profound effect on me. However, today as I write this article, I can honestly say that I have fully recovered.

Before we knew it, we were on our way . . . to load gear onto the Navy Y.F.P.'s assigned for our use. Then on Thursday, One Squadron (we had been organized into three squadrons and six flights) set out early to load Army equipment, cooks, staff-sergeants and the like onto the Y.F.P.'s, and a few hours later the other two squadrons arrived and we were off. As we left the harbour we paraded on deck, acknowledging the commissions of the various Navy frigates and destroyer-es-corts. After approximately three and one-half hours of rolling over the bounding main we arrived at Portland Island. The weather, which had been unsure at the college, had changed to sunny and breezy here at Portland Island. On this great adventure the reputations of many notables were to be established. All shall remember R. J. Williams, star of the

horseshoe pitch; G. W. Brown, sports enthusiast and always ready to contribute baseballs, bats, volleyballs, etc., so necessary for the success of the sports; R. D. Stewart and J. W. Cole, great log-carriers from away back; L. N. Boddy, a wonder on K.P. duty; B. M. Cej, the old man with the cane; and G. W. Dalley, who showed his great potential as a storekeeper and bartender. The immortal stories of others will also remain locked in our memories for years to come and will pass on to our children, who will sit with mouths aghast at our adventures. The days were pleasant (except for one day spent on K.P. duty) and filled with fishing trips, sailing trips on the "Oriole," an outdoor church service, wiener roasts and baseball and volleyball games. We were surprised at the staff's baseball and volleyball team's doing so well, considering the age of some of their members. (To mention names would be fatal.) Although the scheduled overnight bivouac to James Bay was cancelled, the cross-country hike was made. The enthusiasm of the cadet wing should be noted here. We set out at fifteen-minute intervals and a remarkable seventy-three minutes later Two and Three Squadrons arrived in Ganges. One Squadron, who had been extremely lucky, took sixty-eight minutes. After this mighty feat, leave in Ganges was granted and those cadets who did not fall asleep immediately upon arrival at the Y.F.P.'s in Ganges went on leave, toured both restaurants, were entertained by a lady in a car, who was the press, got wet in the rain, and returned at last to the boats. We returned to find a dry little island, which had, during the whole stay, avoided a great deal of rain.

The remaining thing of note was a trip to Vancouver by some cadets on the "Oriole." A grand time was had by all, and some cadets even had showers (their first in six days).

Finally, Wednesday, May 6, rolled around and it was time to leave. The day before, we had taken down all the tents and had loaded them aboard the Y.F.P.'s. Now there remained the cooking stoves and other equipment to be taken, the camp headquarters (consisting of the one house on the island) to be evacuated and all our tents to be packed. Naturally it took much less time to take down the tents, fill the refuse holes and revert the island to what it had been before we had arrived than it had to set up camp. By 1000 we were leaving that sunny little island and were headed back to Royal Roads. First, all we saw were visions of warm showers and beds, then of Royal Roads food and doubling on the circle. Should we go back?

MILITARY WEEKEND

A NAVAL WEEKEND



By C-F/L R. W. Kuntz

Military weekend is the first major event of the college year and the cadets look forward to it with eager anticipation. From their visits to various service establishments and their chats with numerous officers, the cadets get an insight into service life as well as a break from the college routine.

This year's naval trip, which included all the naval juniors and selected seniors from all three services, consisted of a conducted tour of the Naval Base at Bremerton, Washington.

At 0730 hours, Friday, 24 October, 1958, we boarded two Y.F.P.'s and the Oriole, a ketch, and began our trip across the straits. During the voyage each cadet was given an opportunity to steer the Y.F.P. and to change to various Naval formations. We also took

turns at playing navigator even though, theoretically, we usually plotted ourselves in the middle of some island. Naturally we had the rare opportunity of washing the boats from stem to stern. We even tried our hands at cooking. The highlights of the trip included transporting a seaman from the Oriole to the Y.F.P. by jackstay transfer, and rescuing a buoy which represented a man overboard.

In spite of our inexperience at navigating we docked safely at Bremerton at 1600 hours. Within a very short time we were quartered and fed, and were on our way to a football game and dance which were being held as a part of the "home-coming" celebration of the local high school.

The next morning we toured the Naval Base, including the workshops, various ships, administrative buildings, and the vast U.S. Reserve Fleet. The highlight of the tour was a thorough inspection of one of the largest U.S. aircraft carriers, the U.S.S. Hornet, which was at Bremerton being refitted. We were particularly impressed with its magnificent war record. An excellent lunch was then provided for us aboard the U.S.S. Missouri, the famous American battleship. In the evening we were entertained in the Officers' Mess.

Sunday morning we boarded our boats, and with a tinge of regret bid au revoir to our new friends at Bremerton.

Although our questions were numerous, and perhaps sometimes annoying, they were all fully answered and we were shown the maximum of hospitality at all times. But the weekend provided more than entertainment for us. To the juniors it gave a taste of their future life and service. To the seniors it gave a deeper understanding of another service, an insight into the Navy's roles, aims and characteristics. These insights and understandings in later years will help the Canadian Services to work together towards the common objectives, the preservation of world peace.

A VISIT TO R.C.S.M.E.

By C.W.W.O. D. R. Gowdy

TIME 1900 HOURS, 23 OCTOBER, 1958

Royal Roads cadets embark.

Destination: The Royal Canadian School of Military Engineering, Camp Chilliwack, Vedder Crossing, B.C.

This was the annual visit of Royal Roads cadets to a camp or station of each of the

three services. The most fortunate were chosen to visit the Royal Canadian School of Military Engineering. This group consisted of all Junior Army cadets and a mixture of Seniors from all three services. The trip was made by Naval bus to Nanaimo, ferry to Vancouver and Army buses to Camp Chilliwack. It was around midnight when we arrived, and the Officers' Mess very kindly

provided sandwiches and hot, rich "kye" that would please any sailor. Then it was right to bed as our day was to start bright and early.

The following morning we were welcomed to the school by the Commandant of the camp.

We were broken into groups and detailed to a demonstration of their experimental rocket, "The Harp," which drags out 500 to 600 yards of heavy cable behind it. This demonstration left a deep impression on the cadets—and showed them the modern outlook of the Corps.

In the afternoon the cadets were introduced to one of the major tasks of the Engineers, mines and demolitions. Each cadet was instructed and allowed to prepare a small charge of H.E. which he exploded himself. The cadets were stunned and awed by a demonstration of the explosion of different size mines. The effect of this exhibition will long remain in their minds.

That evening the Officers' Mess generously turned the facilities of the mess over to the cadets and provided a party, complete with girls. The mess spared no effort and proved a great success.

However, all good things must come to an end, and the cadets embussed, the follow-

ing morning after breakfast, for the College. The visit was thoroughly enjoyed by all the cadets due to the unselfish different activities. The first activity of the morning was a visit to the Wet Bridging area. There we were shown the various bridging methods and equipment. Then the cadets were permitted to take the controls of the bridging tugs and outboard motor assault craft. The cadets were encouraged to ask any question and to handle all equipment, thus obtaining fullest satisfaction from the instruction.

In the afternoon all the cadets entered a competitive tabloid sports meet with several camp teams. Although the cadets did not win any particular glory, they did demonstrate their drive and keen sportsmanship to the camp. After a delicious supper, the cadets were permitted to go on leave or to watch the movie "High Society," shown in the Officers' Mess.

The following morning was taken up with a rifle competition using the F.N. C-1 rifle. In most cases, this was the first time that the cadets had fired this weapon and they were very impressed with its accuracy and fire power. The attention and effort poured on them by R.C.S.M.E. made all army cadets proud to be a part of the Service.

A VISIT TO R.C.A.F. STATION, COMOX



By C-S/A M. F. Peter

On arrival at R.C.A.F. Station Comox on Thursday evening we were given a warm reception in the Officers' Mess. Here the cadets met the various officers of the station and discussed the R.C.A.F. and life in general at Comox.

Friday proved to be a very interesting and busy day for both hosts and guests. We were

conducted around the whole station and saw how an operational station is run. Among the more fascinating sights were the CF-100 simulator, the Ground Controlled Approach site, and a nearby radar site. Various cadets were given a chance to "fly" the CF-100 simulator and fortunately it wasn't a real aircraft as most of the cadets found themselves landing upside down or sixty feet underground.

Later we were conducted along the flight lines and hangers, where we saw CF-100's, T-33's, Neptunes and Lancasters. The cadets were all well pleased with this part of the tour.

A pistol shoot held for the cadets was won by J/C Draper, who received an engraved cup for his efforts.

In the afternoon we watched a rocket launching, project Comox, a satire on Cape Canaveral. Of course the oil drum never left the ground.

To finish off the day we spent the evening in the Senior N.C.O.'s Mess, where the N.C.O.'s made us feel right at home.

Altogether we had a wonderful time and should a copy of the "Log" reach Comox we would like to thank the officers and men of R.C.A.F. Station Comox for a very enjoyable time.

GUNROOM NOTES

SENIOR GUNROOM

By L/C J. H. Bernard

The room is in darkness. Only through a conscious effort of concentration can one perceive animate figures moving about in the smoky haze, shadows endeavouring to distinguish the sounds of the CWWO parading defaulters from those of Bob Cummings parading—aaahh, yea! In progress is one of the meetings of that elite group, the Senior Rep TV-watching Team, a very exclusive group, restricted to only those 53 stripe-carrying members of the Senior Gunroom.

A nightly watch is maintained on the Gunroom TV set from 1800 to 1900 and again from 2130 to 2230. The only break in this sacred guard comes on weekends, when watches are mounted from 1300 to 2359 instead. Indeed, it is a noble feat to be able to complete a full watch in this paradise of soft chairs and chesterfields. Among those calloused sentinels who have logged larger numbers of hours on duty are: Parnetta, who persuaded the Commandant to grant an additional eight months to help him set his record; Lightburn, who willingly donates his precious pit time to censor Dick Clark's music; Hawkins, who finds it also an opportunity to enlarge his vast grasp on current, shall we say, periodicals.

Nightly at 2130, we find numerous Juniors clamoring at the double doors to gain entrance to this exalted chamber, offering as a sort of persuader multitudinous trays heaped high with buns, and pots fairly overflowing

with cocoa and coffee. Only one of their number has ever succeeded in gaining favour, however. This lucky person, in his Recruit Term, was chosen by the Gunroom Members as their object of affection for the evening and feted in a most glorious manner, consuming each and every gift from his kind and generous superiors with utmost humility and gratitude.

In an effort to distract somewhat from the many superb attractions of the Nixon Block Gunroom, the College has donated the south end of the Cadets' Mess to the whims of the Seniors. As a courtesy for this thoughtful act, the Seniors make use of the comfortable lounge for the consumption of twice-daily doses of galley left-overs, and the consumption of ever-present enormous quantities of mail.

In spite of any efforts, it would be hard to completely replace the affection which each member holds for his Nixon Block sanctuary. Everyone holds fondly the memories of tirades from the Gunroom President, visits by certain members of the Senior Staff, friendly squabbles over whose pants are next, orderly debates concerning the fate of the Royal United States Air Force Maintenance Corps, and the appealing voice of the Gunroom Sweeper calling faintly from beneath a pile of cushions for assistance in moving back the chairs. These events have made the Gunroom our home for the past year and have built up a spirit which will never die. To the new Senior Term we give this spirit, to nurture and preserve in our memory.

JUNIOR GUNROOM

By J/C G. J. Koeller

If the uninitiated were to tour Royal Roads he would have little trouble finding several places of quiet beauty and serenity, and yet the most beautiful spot in the college would not even be brought to his attention. It is in that place of tranquil beauty, that haven of peaceful contemplation (disregarding the piano, record players, radios, TV sets, saxophones, trumpets, drums and guitars) that the junior term gathers to snatch brief moments of enjoyment and pleasure. Here in the Junior Gunroom we again discover the pure bliss of complete relaxation and the wonderful peace of mind gained from the knowledge that here, and only here, are we completely safe from the ever-dangerous cadet officers. From the first day when we doubled aimlessly across the circle, we knew that our gunroom was the place where we were secure from punishments.

What goes on in our gunroom? A great deal actually. At first, the relatively insignificant events, like "Smiley's" letter from his father telling him to keep smiling, were enough to cause a stir in the gunroom. Later, with the introduction of radios and record players, and the discovery of Elvis Presley, our gunroom acquired a new character. Then, finally, in spite of the tight purse-string policy of Rod Newman we obtained a TV set. This has been our latest and greatest addition to the gunroom, and has provided a great deal of enjoyment for everyone. If we could find some way to get Dave Mills and Gerry Koeller away from the front of the set so that the rest of us could see, we would then be approaching the perfect setup.

An alert and unobtrusive observer in the gunroom would probably hear "Little John Laye" still expounding on his last charge, or

Tom Rymer trying to initiate a bold plan of action for some new, fantastic skylark. In the corner, he might hear Len Lee telling jokes in rapid succession, while he beats some unsuspecting opponent at chess, or perhaps see Nick Unitt gaiterless, trying to determine what his next charge will be.

It has been a happy year in our gunroom, and it will be a long time before we forget its panelled walls, the Venture prop, the gang at kye time. We shall certainly regret leaving it when we take our place as next year's senior term.

CLUBS

ROYAL ROADS DEBATING CLUB



R. H. CRANE, L. A. GIBBON, R. W. KUNTZ, PROF. R. F. B. KING, W. S. SMITH, L. N. BODDY, P. C. CLARKE

By C-F/L R. Kuntz

The ability to think quickly and to express himself clearly is a definite and essential asset to any officer. With this in mind, the Royal Roads Debating Club was organized under the directorship of Professor King, the Academic Advisor, and C-F/L Kuntz, President; C-S/A Gibbon, S/C Crane, and J/C's Clarke, Boddy and Smith, W. S., who was chosen vice-president, formed the nucleus of the club.

To create interest and enthusiasm for debating among the cadet wing, an inter-flight debate competition was held. The resolution for the preliminaries was "Resolved that our soft way of life is a greater threat to our security than the Russian military machine." The resolution for the finale was changed to

"Resolved that the U.S. is playing too prominent a role in the defence of Canada." This debate proved to be very interesting because it came at the time the production of Avro Arrow was scrapped. Cartier Flight emerged victorious and the Debating Club received many more supporters.

Although still in its infancy, the Debating Club now has a strong foundation. The topics debated are wide and varied and are all of interest to the cadets. Plans for the future include such ideas as a mock trial and an Oregon style debate.

The members of the debating club feel that this has been a quite successful year, and they are confident that next year the club will be even larger and more enthusiastically supported.

OFFICERS' CHRISTIAN UNION



Seated, left to right—R. H. AMARAL, J. S. ELDER, G. A. SAUNDERS, D. R. GOWDY, R. W. KUNTZ, CHAPLAIN A. J. MOWATT, J. H. FINDEIS, L. N. BODDY, P. C. CLARKE, D. G. STOCKS, T. C. RYMER.
Standing, left to right—R. E. D. UNITT, C. I. MOTTERSHEAD, J. O. BUTLER, P. T. R. PAYNE.

By J/C I. Mottershead

Each Sunday evening from 1800-1915, the Royal Roads Officers' Christian Union, a group of about fifteen cadets, both seniors and juniors, gather at Chaplain Mowatt's residence to discuss selected topics. The O.C.U. exists in all three services of Canada, the United States, Britain and the Dominions, and groups meet all over the world to assist in bringing Christ to the forefront in the lives of officers. The meetings at Royal Roads are conducted

in a similar manner to these groups. After a short devotional period with the chaplain, the subject of interest is analyzed and debated. The themes have been widely varied, from such broad topics as "Juvenile Delinquency" to more philosophical topics such as "What Is God?"; from such challenging topics as "Colour Prejudice" to more day-to-day problems as "Interdenominational Marriage." Club officers are: President, C-F/L Kuntz; Vice-President, J/C Mottershead; Secretary-Treasurer, J/C Boddy.

AMATEUR RADIO CLUB

ON THE AIR AT VE 7CSC

By C-S/A L. A. Gibbon

The Royal Roads Amateur Radio Club has been quite active during the past year, concentrating on radiotelephone operation. Although small, the club has been quite busy. A new beam antenna was erected which improved transmission and reception considerably. Stations in Japan, Uruguay and Northern Ireland were worked during the year as well as many eastern Canadian and American stations.

The station has a fifty-watt CW (morse code) transmitter which operates thirty watts on radiotelephone. The receiver is loaned to the club by the R.C.N.

One amateur found that the hobby is not only of technical interest. A contact with a YL (young lady operator) in Winnipeg netted him an invitation to her home during summer training. Another service which the club tries to perform is "phone patching" officer cadets' homes. This is a method of combining radio facilities and the local telephone service to enable the cadet to talk to friends or relatives back in his home town.

The Radio Club provides a recreational outlet for officer cadets with an interest in electronics. As yet no young enthusiast has perfected a 3-D TV set or a cadet officer detector, but they're still working on it and "hope springs eternal."



Standing (left to right): P. B. Murphy, G. A. Tyler, R. J. Kovacs, W. Voort, R. M. McGimpsey, O. L. McCarthy, T. N. A. Jensen, M. W. Hones, E. E. Goski.
K. A. Smeo, L. G. P. Lee, K. D. Hamakawa, R. L. Sykes, K. J. Parkes, A. W. Pchajek, F/L K. E. Lewis, A. T. Downs, R. P. E. Miners, D. B. MacKenzie, F. M. Bucknum, J. S. Ellis, R. S. Bryant.

FLIGHT NEWS

CARTIER FLIGHT

By S/C D. A. Harris

"Cartier" Flight will have a flight party in the canteen at 2130! Juniors pay! As soon as these words are heard, the gravel pounds to silence the tumult created by the spirited Seniors.

This has been a good year for Cartier Flight. Throughout both terms, the flight members have shown lots of spirit and determination. This determination has been coupled with drill on the parade square, which led to a first-place standing in the Wisemen Cup race first term, and accounts for our lofty standing at present.

Even though the flight provided their share of cadets on representative teams, Cartier Flight still managed to remain in the top half in inter-flight sports, led by C.W.C. Cavanagh and Flight Leaders Dalley and Antonsen, the flight could not fail.

There are still certain individuals who just

love to polish cannons and who can't seem to get along without ankle supports. But, everyone in Cartier has had fun and we will always remember in future years: "the tiger twins," Antonsen and Gilbert; George "Diane" Dalley; "Rudy" Kovacs; "Mr. Five-bars" Cavanagh; "Jazz" Lytle; Parnetta and his "D"-weeks; Harris and that certain girl; and Landry and that Christmas dance; "Skylark-ers" Buckmun and Bryant; "Rocky" Brown; Gaiter-twins" Bezeau and Cej; "Roberta" Calloway; Amaral with his Australian flag; "Keener" Allen; Butler and his Saturday night date; Bauer and "Janet"; Boddy always debating; "Rifle" Carpenter; and last but not least, Clarke and Castonguay and their general keenness?

As we leave the college this May, the graduating Seniors wish our future Cartierites the best of luck in the coming year. To you Juniors we leave the responsibility of keeping up the tradition of Cartier Flight.

FRASER FLIGHT

By S/C R. G. Hawkins

Though Fraser Flight is deemed the lesser half of "the fun squadron," it lacks nothing in enthusiasm, loyalty and spirit.

Following last year's example, Fraser has been a mainstay of the representative teams. Such people as Sykes, Inglis, Saunder, Lyle, Dueck, Draper, Dvorak, East, Clegg, Cole, Fraser, Findeis and Ellis upheld the flight in representative sports and their loss was keenly felt in flight sports.

The remaining members, however, took the flight on to great heights in volleyball, water polo and basketball.

This year the flight was considerably changed and many faces disappeared. We lost our "gaiter kids," but gained a certain

"Red-headed Yahoo" from Vancouver Flight, who created havoc wherever he went.

As in the preceding year, Fraser Flight also fostered multi band-members and blue armbands. Sometimes we even mustered three ranks for morning Wing Parade, but this was rare.

Fraser Flight Juniors this year are indeed a motley crew of skylarkers, scholars and good-humour boys; two especially are always happy. However, leadership in the humour department must be awarded to our excellent librarian, "Whooper Crane."

Seriously, though, it has been a very rewarding and enjoyable year, one in which flight spirit abounded. Let us hope that next year will be even a finer one than this one has been.



Back row, left to right—P. C. Collaway, R. S. Bryant, B. M. Cej, L. N. Boddy, C. L. Castonguay, R. H. Amaral, J. O. Butler, G. W. Brown, J. G. Allen, D. M. Carpenter, F. M. Bucknum, P. C. Clarke, M. V. Bezeau.
Front row—J. A. M. Landry, A. E. Parnetta, M. J. Gilbert, M. W. Cavanagh, Lieut.-Cmdr. T. H. Ellis, P. G. M. Antosen, G. W. Dolley, E. R. Lytle, D. A. Harris, J. R. Kovacs.



P. G. M. Antosen, G. W. Dolley, E. R. Lytle, D. A. Harris, J. R. Kovacs.
H. C. Engler, J. S. Ellis, G. E. Gillespie, E. G. Dueck, D. Clegg, J. W. Cole.
Front row—G. A. Saunders, N. Inglis, L. A. Gibbon, R. L. Sykes, Lieut.-Cmdr. T. H. Ellis, R. W. Kuntz, D. R. Gowdy, G. J. Dallimore, R. H. Crane, R. G. Hawkins.

CHAMPLAIN FLIGHT

By J/C E. Lawder and J/C J. W. Laye.

Champlain Flight got off to a good start at the beginning of the 1958-59 year under the capable leadership of Mike Sullivan. At that time the spirit in the flight reached unequalled heights, which have been maintained up to the present time through the efforts and patience of Dave Smith and Ed Exley, Second and Third Term Flight Leaders.

Champlain Flight was the largest in the wing, having lost only two cadets throughout the year. The loss of some of our better athletes to the representative teams hampered us, however, but we came through in true Champlain Flight style to place first in the cross-country, the swimming meet and the basketball tournament. We placed second in the track and field meet and in the race for the Aggregate Shield, and held respectable positions in rugger, soccer, volleyball, marks-

manship and water polo. Our showing in sports commanded respect from the remainder of the wing, although it must be admitted that we lacked something on the parade square.

Although the wording of this article would indicate that our flight contains "personages of great literary skill," our flight was given the probably appropriate name of the "Happy Gang." From our group of mediocre scholars, however, emerges Vic Thomas, who placed first in the Senior Term with an average well within the First Class Honours range.

Our thanks go out to the members of the staff who assisted us in establishing this fine record. We would like to take this opportunity to wish the Champlain Flight of the future the best of luck in all their endeavours.

"We're the power pack of might,
We're the best, we're Champlain Flight!"

MACKENZIE FLIGHT NEWS

By L/C D. T. Lightburn

"Mackenzie Flight will fall in for drill at . . ." These words were heard many times this year, but what the flight lacked in drill they made up for in sports.

Mackenzie stood high up in the final standings of the interflight sports program, gaining a first in rifle shooting, seconds in volleyball, hockey, swimming and boxing, and placing either third or fourth in the other sports.

Aside from the interflight sports, nineteen out of the twenty-two members of the flight were on a representative team for Royal Roads this year. For the soccer team, we had MacDougall, Lightburn, Mills and Mottershead; for the rugger team, Pellow, Newman and Mackenzie, and for the rifle team, MacDougall, Newman, Nichols and Miners. Present on the cross-country team were Grott, and McGregor; on the fencing team, Munday and Mulder, and on the water polo team, Porter, Grott and Pellow. The flight was represented by Money, McGimpsey, Mackay and Mackenzie on the boxing team; by Porter and Pellow on the volleyball team, and by Shepherd on

the squash team. Finally, we had Hopkins, who was captain of the basketball team this year.

Certain members of the flight pursued rather unusual hobbies this year: MacDougall, attending parties held by other flights; Porter, occupying the front seat in all physics classes; McGregor, getting two haircuts a week; Murphy, receiving weekly "care" packages; Mills and Miners, hoisting the ensign upside down; Mugford playing water polo by football rules; Munday acting as "Flag Officer" for a week; and Pellow sending code messages back east on the back of his letters.

After our inter-flight sports victories, the "Goofers" often held flight parties. Entertainment was supplied by "Elvis" Neil, "Traverse Right" Hopkins and "Digger" MacDougall. Left-over cigarettes were usually distributed quite evenly between Shepherd and Money. The parties ended with the familiar flight yell, "Bones, ligaments . . ."

Altogether, it was a very good year for Mackenzie Flight, thanks to the guidance and leadership of C-F/L's Money, MacDougall and Hopkins and C-S/L Pellow.



Back row, left to right—G. A. Kennedy, G. J. Koeller, L. R. Larsen, J. E. Laye, J. S. Hemenway,
J. W. Lowthian, M. L. Kochanski, E. E. Lowder.
Middle row—P. A. Jacob, L. G. B. Lee, N. W. Hass, D. L. McCarthy, T. N. A. Jensen, K. S. Hayes,
M. W. Hewes, R. F. Kanggeisser.
Front row—R. H. Thomas, V. R. Thomas, J. D. Sutherland, E. Exely, Capt. B. Baile, W. M. Takaki,
K. J. Parkes, D. C. Smith, M. B. Sullivan.



Back row, left to right—A. J. Nichols, D. G. Mills, W. J. Neill, S. J. Mugford, C. I. Mottershead.
Middle row—R. E. MacKay, M. C. McGregor, D. B. Mackenzie, R. D. Newman, R. M. McGimpsey,
P. B. Murphy, D. L. L. Mulder, R. P. E. Miners.
Front row—D. B. Porter, S. C. Shepherd, J. R. MacDougall, A. D. Hopkins, Capt. B. Baile, J. C. Pellow,
R. W. Munday, S. A. Money, D. T. Lightburn, M. J. Grott.

HUDSON FLIGHT

Long after the cheers have died away and the other flights have forgotten the glory of carrying off sport championships, the drill staff will remember their pride and joy (?) Hudson Flight. Our secret to success in the field of drill was the four file, two rank system, and the keen participation in extra drill.

Although Hudson's early victories were few and far between, nevertheless we rallied our forces to place high in water polo. Unfortunately, too late in the year, we switched from Wheaties to Hudson Corn Pops, and perhaps this accounted for our slow start. However, during our darkest days (and they were dark) our spirit never waned. We always came back for more.

During the first term, Hudson was under the thumb of our capable Flight Leader, Mr. D. Anderson, who also carried us through the

final term. During the interim we were led by the equally efficient Mr. B. Clark. Had we ten more like him, we would have finished on top of Flight Sports. Hudson this year is the boiling pot of North American, composed of cadets between those great bounds of New Jersey and Dawson Creek.

All of the flight waited in anxious expectation at every Hudson Flight party, for the next presentation of the Hudson Corn Poppers Little Theatre Group. Whether it be Wing Parade or Duty Flight, we put a maximum of effort into every thing we undertook. We hope that next year the new Hudson will carry on the traditions of the old, and we wish them the best in years to come. H-U-D-S-O-N, Hudson!

By J/C M. W. Smith and
J/C R. D. Stewart

LA SALLE FLIGHT

By S/C A. T. Downs

LaSalle Flight has had a wonderfully successful year in 1958-59. Militarily, LaSalle ranks with the best. This was shown when it won the first division of the Wisener Cup. The first term Flight Leader, Al Pchajek, rose to Squadron Leader for the second and third terms, and was ably replaced by Bob Craig as Flight Leader for this same period. Mike Peter seconded the helm as Squadron Adjutant for the last two terms.

As for sports, LaSalle has again excelled, finishing at the top of the standing for the Grand Aggregate Shield. With many of its members on "rep" teams, LaSalle Flight has been handicapped in many competitions, but flight spirit and good old-fashioned drive nearly always pulled her through on top.

We, the outgoing Seniors, wish our Juniors all the best, and feel sure they will lead LaSalle on to an even more imposing position next year.



Back row, left to right—W. S. Smith, M. W. Smith, R. A. Rutherford, P. T. R. Payne, B. E. Pattison, F/L K. E. Lewis, R. L. Rowlett, R. D. Stewart, T. M. Robinson, T. C. Rymer, K. A. Smea, R. L. Spickett.
Front row—E. E. Goski, G. A. Griffith, J. H. Bernard, D. E. Fallon, D. T. Anderson, D. A. Thomson, B. D. Clark, J. S. Elder, W. A. Cook



Back row, left to right—R. I. Williams, G. L. Stowe, S. A. Yakimishyn, R. J. Wreford, R. E. D. Unitt, G. C. Tovell, W. Voort, R. C. Waller, W. J. Taylor, G. A. Tyler, D. H. Warr, D. G. Stocks, G. L. Walt.
Front row—C. G. E. Vardy, K. D. Hamakawa, J. M. Buchan, M. F. Peter, R. W. Craig, F/L K. E. Lewis, A. W. Pchajek, W. S. Hammond, A. T. Downs.

THE BAND



Back row, left to right—M. L. Kochanski, R. M. McGimpsey, B. E. Pattison, G. L. Stowe, R. D. Stewart,
R. C. B. Fraser, R. D. Newman.
Front row—R. J. Wreford, J. H. Bernard, W. A. Cook, W. S. Hammond, J. E. Laye.

By J/C R. D. Stewart

This year's band, under the able direction of C/F/L Wayne Hammond, L/C John Bernard and J/C John Bauer, stormed its way through the 1958-59 term. Despite the fact that most of the musicians were in R.C.N.H., Sick Bay, or on excused, they soon showed great improvement over the discordant group who assembled for the first time last September.

The bugle section was composed of a weird and wonderful selection of cadets. To C/F/L Hammond's great joy, most of the buglers realized that they were playing bugles and not French horns or triangles. A few of the personalities in this select group were: S/C Whitney Cook, who disliked the idea of being C.O.O.; J/C Gerry Stowe, who cultivated a planter's wart; J/C John Laye, who was too short to do anything else; J/C Rod Newman, who had been "Shafted" into the band in one of his weaker moments; J/C Earl Pattison, who found, much to his surprise, that the "Cute Little Thing" was a mouthpiece; and last, but not least, J/C Morris Kochanski, who constantly forgot sunset, and who was the only bugler who could goof up the "G."

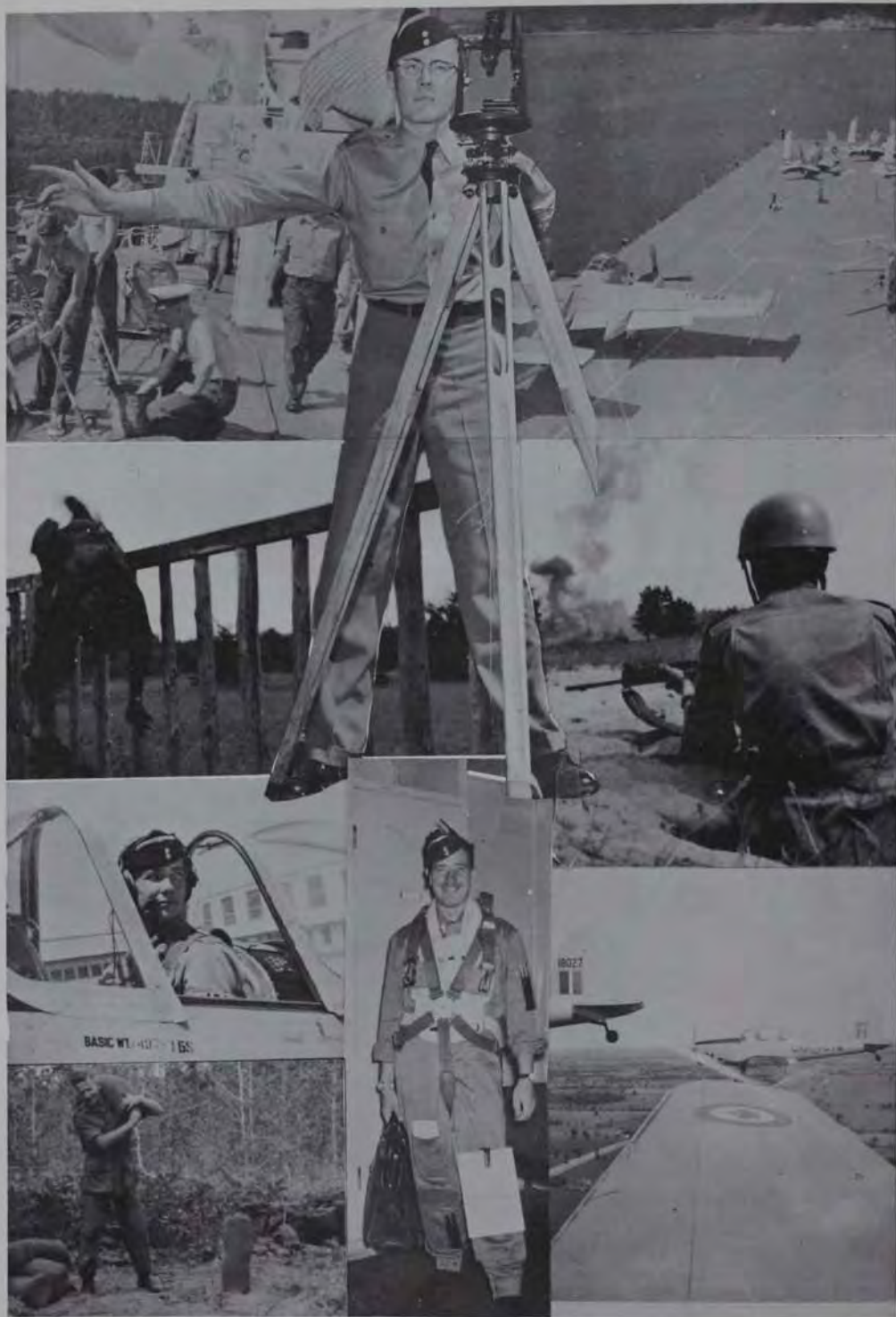
It seemed that the buglers never did learn the introductions to their pieces. It was always a surprise to the drummers to find they were playing "Semper Fidelis" and not "North Atlantic Squadron" (not to be confused with either No. 1 or No. 2 Squadrons).

The drummers were a fine group in their day, with J/C Robin Fraser leading them to

many victories over the bugle section. J/C Rod Wreford's intentions to stay in the band were often under suspicion, and despite the fact that J/C John Findeis had never played the bass before, he had learned by Christmas that one drum stick per hand was by far the best arrangement. This group's favourite trick was to give an elongated beat-off which completely baffled the poor buglers. However, Sunday wing parade was the drummers' delight. Requested to dream up a few patterns to speed the inspection party along its solemn way, they came up with something that started with four loud, spine-jerking crashes on the bass drum, followed by a seven-beat roll on the side-drums, which sounded more like the prelude to a guillotine scene than an inspection march. They then proceeded in playing so slowly that the inspection dragged on even longer than before.

By the end of the year, J/C Stowe's bugle was a mass of small notches, each one representing a duty cadet who had fouled up sunset. J/C Bob Stewart, an ardent (?) drummer, still believed that Mr. Bernard threw fingers every morning to decide the number of circles to hand out on inspection.

All in all, the band was made up of a group of really decent fellows whose "raison d'être" could be inferred from the following remarks in band practice: "Maybe we don't lead them, maybe we don't inspire them, but brother, do we confuse them."



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SUMMER TRAINING

R.C.N. SUMMER TRAINING REPORT

By S/C R. W. Munday and
C-S/L R. L. Sykes

"Ontario's Ho!!" It is commands such as this and pipes such as "Crane Driver, Close Up," "Liberty Men lay aft" and Cadet Don Sutherland's "For'd Brow" that will stick in our minds for years to come. These are the memories of our first Summer Training Phase.

There was never a dull moment on the "O" and the days were crammed full of classes, P.T., Communications, duty watches, studying and even a turn at cleaning stations. To add to the confusion of this routine, Ralph Sykes was always screaming "Fall In!" and requiring that we be there five minutes before five minutes before . . .

After three interesting weeks at Royal Roads we joined H.M.C.S. Ontario, slipped, and proceeded to Renne Sound. Here, after being introduced to Sea-Boat drill, we endeavoured to become familiar with each of the five-hundred and fifty-five feet of Canada's only cruiser.

Soon we were joined by ships of the Second Canadian Escort Squadron and proceeded to Prince Rupert, where we received a lecture on "Why Ships' Companies Should Not Have Inter-Ship Battles in the Main Thoroughfare of Prince Rupert!"

Next—due south to San Francisco. About one hundred miles off shore and one hundred and fifty miles from San Francisco we picked up a private yacht in distress. The next morning we sailed under the "Golden Gate Bridge" and berthed at Treasure Island. After a brief stay, long enough for Bob Munday to acquaint himself with a certain fair damsel, we were off again.

Powell River and then Ganges were next on our port-of-call list. At Ganges the townspeople arranged a couple of parties for the officer-cadets on board, and before we could escape Phil Antonsen's interest was already deeply occupied with the presence of a certain young lady. This pleasant disease was becoming contagious.

And then Esquimalt! Here we took part in the first Canadian Fleet Review and had a week of eating box lunches at the A.B.C.D. School. Before we left, we joined with Venture, U.N.T.D. cadets, American and British Midshipmen in attending the Command Midshipmen's Ball. Following this we competed with Venture and the U.N.T.D. in an early morning regatta, which our expert sailor, Tex Thomas, unfortunately could not attend.

Away again to Vancouver! The intense heat and excitement of fleet-week activities provided a welcome excuse for daily "Make and Mends."

To get away from this fast pace we again turned south, destination—Santa Barbara. By this time of the summer, money was getting low so Gord Hawkins and Dick Crane rented a "two-seater" bicycle and toured the area. A pedalling good time was had by all! Also at this point lucky old Norm Inglis moved out of the mess and into a cabin. This resulted from a slight outbreak of influenza among ship's company and the officer-cadets.

All too quickly the summer was drawing to a close and we were homeward bound. We left the ship and returned to Royal Roads for examinations and "sun-bathing." For the first time all summer Ted Cook lost his natural (?) green color.

ARMoured CORPS

By L/C G. J. Dallimore

"Rrrr . . . clank, clank . . . Traverse right . . ." These are typical armoured sounds, but they were not for us to create during our first summer's training at the Royal Canadian Armoured Corps School, Camp Borden, Ontario. Why? Because our training was that of the infantryman, of the man who forms the nucleus of our defense setup. The summer's training was not wasted, however, as it showed us the great problems the mud-slagger must face, and the tremendous advantages presented by the use of armour.

Out of the twelve "men" who completed their first phase in No. 5 troop, seven were from Royal Roads, the remainder being from the other mediocre colleges down East. With Graham Dallimore, Ed Exley, Don Fallon, Jerry Koeller, Dave Lightburn, "Digger" Bob MacDougall and Dave Thompson being the backbone of the cross-country team, "mighty five" had no problem clinching O.C.S.'s grueling cross-country run. In another competition, No. 5 troop walked away with first prize in the mock obstacle course held on Cadet Weekend.

As for the training, it was unanimous among us that we could not have had better instructors if we had the whole Canadian Army from which to choose. Perhaps we were pushed hard during the week at Meaford, which climaxed our training, but we really

relaxed during the three-week wireless course that followed.

Even with the ninety hours on the parade square and the high cost of living during the summer, the big seven of "five" look forward with eagerness to the next time we enter the gates of Camp Borden.

R.C.A. SUMMER TRAINING

By L/C E. R. Lytle

On May 21, 1958, cadets from the Canadian Services Colleges arrived at Douglas, Manitoba, in the midst of a violent sandstorm. After some confusion we arrived at the Royal Canadian Artillery's one and only Camp Shilo and here proceeded to locate our quarters.

The twelve-week infantry course soon got under way and everyone was delighted to learn that we would participate in numerous drill periods during the first two weeks. "B" troop, composed of COTC, soon learned how to manipulate their feet when the order "stand at ease" was given. Much of our time during the first half of the summer was spent in the

classroom, but later on we had one week firing all types of infantry weapons (the rocket launchers provided many throbbing heads), one week's truck driving, and lastly, participation in a very interesting two-week scheme.

During the week-ends special patrols were sent out to Winnipeg, Clear Lake or Brandon. The "A" troop bridge team spent many nights happily tramping one another while the golf enthusiasts enjoyed tearing up the greens. The summer came to end all too soon with a lively party held in our mess in which all in charge of our training were present.

We left Camp Shilo looking forward to next summer's artillery training.

R.C.S.M.E.

By S/C M. J. Grott

Since our arrival at Royal Roads we had heard continually about the glorious times that our seniors experienced during summer training. As a result, to most of us, nothing was looked forward to as much as our first phase training at our respective corp schools. Camp Chilliwack, the home of the Engineers, saw two Royal Roads cadets arrive on the 22nd of May to take up what they thought would be a slack summer course. It did not take long, however, before this thought disappeared. Immediately we were introduced to our living quarters, which were tents! We had expected to "rough it" but not to be required to keep mere tents up to such a high standard of neatness.

Under the stern guidance of our Sergeant-Major and Troop Commander, we soon learned the life on an infanteer: up in the morning at 5:30; out on the parade square with our no-

torious Sergeant; out on the range or else crawling through a field of thistles; digging trenches; running through the obstacle and bayonet assault courses. Near the end of the summer we finally got into some of the work of the Engineer, which consisted mostly of mine warfare and field defences, but we did no bridging at all. Despite its exhaustive nature, however, the training was interesting and practical.

The week-ends were probably the most enjoyable part of the summer. Vancouver was close by and Cultus Lake provided a nice place to swim and—other things. The mess life at R.C.S.M.E. was also excellent, with dances provided every week-end.

The march-off parade highlighted and brought to a close the memorable summer. During this ceremony ex-Royal Roads Cadet McHale was presented with the sword for the "Best Cadet, Second Year."

SUMMER TRAINING—R.C.S. OF I.

By S/C C. G. Vardy

John Buchan	Dick Kuntz
Bob Craig	Jon Pellow
Stu Elder	Dave Smith
Dave Gowdy	Glen Vardy
Wayne Hammond	Herb Widdifield
Don Harris	

Early last summer the enlightened eleven mentioned above departed from the hallowed halls of Royal Roads to the place where THE fighting men of Canada are trained, the Royal Canadian School of Infantry, Camp Borden.

Little time was wasted before we began training. The night after we arrived we were

holding a defence position and were subjected to patrolling action from second phase. The pace never slowed. We gained considerable skill in using the rifle, pistol, sub-machine gun, rocket-launcher and grenades. We learned to march with full webbing until it felt like it was full of lead; we learned to stalk in ground as open as the desert, and most of all, we learned to laugh when life was hard.

The first six weeks were spent in classrooms, on the square, or on the ranges. Bob Craig showed such proficiency here that he passed the range test on the pistol with the amazing score of ?? At the end of these six weeks we got our chance to test our skill and endurance. We were dropped off in some of the most miserable country known to man, given a compass and map and told to march to a point about thirty miles away. We marched at night only and we had three nights to finish. The ten miles at night was purely theoretical. To avoid enemy patrols, possible ambush and other danger areas it was necessary to march about twenty miles a night. About three-quarters of the party finished the march. On the morning of the third day we saw the trucks that were ready to take us back to that beautiful "H"-hut we called our summer home.

After more classroom work on atomic, bacteriological and chemical warfare, water purification, section tactics and platoon tactics, we were in the field again, this time to construct and man a platoon defensive position. This in itself is a fairly easy assignment,

but when half the platoon is stringing wire, a quarter of it on patrol and the other quarter standing guard, there is not much time for non-essentials such as sleeping and eating. Roads proved itself resourceful again, however, and we learned the art of sleeping while standing up and during any break in routine.

Although the routine was full and hard, it appeared to the responsible officers of R.C.S. of I. that someone, somehow had managed to save some energy. One night cannons were moved, signs were placed in unusual positions and the fire picquet saw shadows moving all over the camp. The trouble was soon traced and next morning Roads was pushing cannons back in place. Skylark anyone?

When a weekend pass was granted to the platoon, our hut was vacated and the weekly exodus to Ottawa, Toronto, London, Galt, Cobourg and other places too numerous to mention began. When the platoon returned for company drill on Monday morning it was noticed by our N.C.O.'s that the other platoons drill came dangerously close to surpassing ours. Oh! What a weekend!

The words of the school commandant, Colonel Dextre: "I've seen too many young officers go to their units untrained and not prepared to command a platoon. The officers I train will be ready," sum up the spirit that pervaded R.C.S. of I. this summer. It is certain that infantry officers from Camp Borden will be outstanding even in such a magnificent corps as the queen of battle, the infantry.

AIR FORCE SUMMER TRAINING

By C/S/A L. A. Gibbon

Officer cadets in the R.C.A.F. spent a very enjoyable and rewarding summer, training in various air and ground crew trades. First phase courses varied considerably in duration and location.

The pilots took a six-week elementary flying training course at the R.C.A.F. Station, Centralia, Ontario. Early in June, John Bernard, Earl Goski, Al Pchajek, Jerry Saunders, Stan Shepherd, Bill Takaki and Vic Thomas took to the air at the controls of the "Mighty" Chipmunk. Only once did they manage to approach the phenomenon of negative ground speed while maintaining air speed during high winds. They claim its quite a trick to be able to land an aircraft backwards. All work and no play, however, makes Al a dull boy, so the pilots spent leisurely weekends at Grand Bend and Lake Huron.

While the pilots frolicked amongst the cumulus and stratus clouds, the observers were busy in the class-rooms of number Two

Air Observer School at R.C.A.F. Station, Winnipeg. An interesting summer studying basic navigation principles was spent by Don Anderson, George Dalley, Larry Gibbon, Marcel Gilbert, Rudy Kovacs, Marc Landry and Dave Porter, who have come to the conclusion that getting lost in the air is one of the easiest things to accomplish. Two solutions to this problem were discovered—one is to be an air interceptor navigator and the other a radio officer. In the first case you are guided by the fighter cop on the ground and in the second case you just bash the key. Relief from the routine of classes was obtained on weekend trips to Kenora, Clear Lake or Edmonton. Meanwhile on the ground, the "Tech-types," as they are called, spent a successful summer at their varied occupations. Tony Downs went to R.C.A.F. Station Penhold, Alberta, for contact training as a construction engineer surveying and building new station facilities. Back east, Bryon Clark was studying the P-3350 and other natural and mechanical phenomena during his aeronautical engineer-

ing course at the R.C.A.F. Station Aylmer, Ontario.

At the R.C.A.F. Station Clinton, Ontario, Sid Money was always short of \$'s as he blended his armament course and social life into an enjoyable summer. On the same station, Dick Hamakawa, Ken Parkes and Mike Peters studied the condensers, coils and resistors, etc., that constituted their study of telecommunications. The "teck-tel" fellows found their training rewarding both academically and socially.

One of the highlights of R.C.A.F. Summer Training was the parade at Winnipeg com-

memorating the completion of R.C.A.F. training of air crew personnel for North Atlantic Treaty Organization countries. The pilots and observers devoted a week of summer training to combine their parade square talents to make up the R.C.A.F. Guard of Honour. The parade was televised and also attended by many high ranking governmental and military officials of the NATO countries.

These are a few of the main aspects of first phase R.C.A.F. Summer Training, 1958. As we look back we have many happy memories. As we look forward we see a challenging future as officers in the R.C.A.F.



CPO W. A. Kelly, S/Sgt. F. S. Dodd, Sgt. T. Jones

LITERARY



ROYAL ROADS: YESTERDAY AND TODAY



HATLEY PARK

By C-W/C M. W. Cavanagh and
S/C M. Sullivan

Royal Roads is situated in a tree-surrounded re-entrant on the coast line of Vancouver Island. The college looks out upon a salt-water anchorage from which it takes its name, and across the straits, in the State of Washington, one can, on a very clear day, have an unparalleled view of the snow-capped Olympic Mountains. The name Royal Roads was originally given to the anchorage in front of the college by a Spanish sailor, Lt. Don Manuel Quimper. In 1780 he crossed these waters to Albert Head and he named them Veldez Bazan, which means Royal Roads.

The grounds upon which the college now stands were purchased originally by the Honourable James Dunsmuir. He had made a fortune in coal mining near Nanaimo and he wanted the property to establish a family estate. The first house on the grounds stood on what is now the parade square, but this house burnt to the ground while the family was in England.

When Mr. Dunsmuir returned, he started the construction of the main house, which is known as the castle. He engaged a noted architect, Samuel McClure of Victoria, and at one point in the construction of the building he was reported to have said, "Money doesn't matter . . . just build what I want."

The original plans for the estate called for 232 acres of ground and the cost of construction was to be approximately \$300,000. When finally completed, the estate covered 650 acres and cost \$1,000,000. At first, Mr. Dunsmuir planned to do the landscaping himself with the help of Mrs. Dunsmuir. Later he relented, but maintained an active interest as nearly 100 Chinese gardeners worked on the grounds to make lawns, gardens, rockeries and arbours from what had been an undisturbed forest.

Finally, ready for occupation in 1909, the estate was functional as well as beautiful. Included in the buildings on the grounds were a model dairy, a large stable, three silos of 100 tons capacity each, a Chinatown for the 100 gardeners, a large refrigeration plant and even a slaughter house and smoke house. To the west of the Japanese Gardens, which contained three privately stocked fishing pools, was a beautiful conservatory filled with \$75,000 worth of white orchids, and south of Belmont Drive stood a reserve water tank. In the middle of the depression, the owner had a large stone fence built around the estate for the fabulous price of \$75,000.

The castle itself was completed in 1908 and the Dunsmuirs occupied it shortly thereafter. It was sumptuously finished with oak and rosewood panelled rooms and teak floors.

The ceilings were intricately decorated with white plaster and the windows on the lower floors were glazed with a special stained glass made to Mr. Dunsmuir's specifications.

Mr. Dunsmuir died in 1920, but his wife and daughter continued to reside in Hatley Park until 1937, when they both died. For the next three years the estate was looked after by a caretaker. In 1940, the Dominion Government purchased the million dollar estate for a paltry \$75,000 in order to set up a Naval Officer Training Establishment, which opened in 1941.

Unfortunately, when the grounds became government property, many of the buildings were razed. The only structures which remained were the Castle itself, the model dairy and the stables.

Today the Castle is used as the main administrative building for the college. The Sick Bay, a well-equipped Library, Officers' Wardroom, as well as a dining room for the staff, are also located in the Castle. The original model dairy has now been equipped as an Engineering School, with various shops and draughting rooms being located there. The old stables have been converted into a Military Training Divisions building. Occasionally informal parties are held in this building, and during part of the summer Naval Cadets live on the premises. Recently, the Chief Petty Officers' Mess was moved from the Castle to this building. The gatekeeper's house is now the residence of the Executive Officer, while the two houses occupied by the servants are now the residences of the Commandant and the Director of Studies. These are the only original buildings which are now used by the college, but a number of new buildings have been added.

The Grant Block, which was opened in 1942, now houses the Officer Cadets' Mess, classrooms, offices and laboratories. The Quarterdeck is used for indoor parades, dances, and as a lecture hall for the entire Cadet Wing, and on Sunday it is rigged for church services. Originally the office for the Staff Officers was located to one side of the Quarterdeck, but these rooms are now used for the Cadet Office. The old Senior Gunroom has been converted into the "Wing Office" for the Staff Offices, and the old Junior Gunroom was incorporated into the Mess, the construction work being completed in the fall of 1958. Originally there were dormitories on the third deck of this block, but these have been converted into classrooms, with a large Chemistry Laboratory being added at the east end, and two draughting rooms on the west end of the Grant Block since February of 1956, when the Nixon Block was opened.

The Nixon Block, which is located immediately to the west of the Grant Block, is the



HATLEY PARK, VICTORIA, B.C., 1929.

living quarters of the Officer Cadets. Each Officer Cadet has his own cabin, which is equipped with a bed, washbasin, study lamp and desk, a large closet and chest of drawers, bookcase and easy chair. These cabins are quite large and they are very comfortable. Two Gunrooms, or common rooms, are also located in this block, as well as a canteen for Officer Cadets. The block is four storeys in height, and is constructed with a steel frame and poured concrete. There is a total of one hundred and fifty-six cabins.

The last addition to the college was a swimming pool. This pool is an indoor pool located to the west of the gymnasium that was built when the college was originally opened. The swimming pool, which is of very modern design, is twenty-five yards in length. Of course, the water is kept quite warm, around 75 degrees. The building housing the pool contains a large balcony overlooking the pool, changing rooms, P&RT offices, sport stores and two squash courts. A large tunnel connects the swimming pool and the gymnasium. It was in the Spring of 1958 that the pool was completed and opened, and it has proved to be a great asset to the college.

One can see that the college is very adequately equipped to handle the Officer Cadets who are being trained here. Each year brings a few changes in the physical aspect of the college, which all help to increase the efficiency of the college.

THE STORY OF "THE UNIFORM"



By C-W/C M. W. Cavanagh

The uniform worn by the Officer Cadets of the Canadian Services College is one of the most colourful worn by any of the services. It has often been laughed and scoffed at as being uncomfortable and impractical. Yet the story of this uniform is rich in tradition and it is one which should be worn with great pride. However, before discussing the actual uniform, let us go back in time and trace its development.

Before the advent of national standing armies, armies were composed of levies from the shires and estates and various groups of mercenaries. The nobility, earls, lords, counts, etc., simply gathered together their serfs and yeomen and, under the banner of the lord, they united to form part of the king's armed forces. Since these "soldiers" dressed at their own discretion, it was difficult to distinguish friend from foe.

In time, as the kings gained greater power over their nobles and began to maintain permanent forces, the archers, pikemen and musketeers began to wear distinguishing tunics, badges, plumes or accoutrements. In the English army red was a popular colour, and in 1585 Henry VIII began to dress his Royal Body Guard in uniforms in which red predominated. Other colours were worn by other regiments, but by the end of the 16th century red was recognized throughout Europe as being the predominate colour of the English uniform, just as white was to become the "traditional" colour of the French.

During the period of the British conquest of Canada and later, the American Revolutionary War, British Infantry Regiments were, for the most part, dressed in scarlet tunics with various "facings" of blue, white, yellow, etc., on the cuff, collar, etc., to distinguish regiments and corps. But certainly the "Red Coats," as they were called, were by this time well known on this continent by colonials and natives alike as British infantrymen.

In Canada, during the century from the capture of Quebec to Confederation, the militia was trained and equipped by the British army. Quite naturally the Canadian militia regiments which fought side by side with British forces during the War of 1812 wore British uniforms and fought with British arms and artillery. This practice continued even after the British forces were withdrawn after Confederation, not only because the Canadian militia was commanded by British officers for another half century, but also owing to the custom and tradition which had grown up during the decades of very close association between the British and Canadians.

It is not surprising, therefore, that in 1876 the dress regulations for the Gentlemen Cadets of R.M.C. included this same scarlet tunic, the uniform being patterned after that of the Regular Infantryman. It was very similar to the Full Dress of the British Infantry of the Line, but the uniform worn then did not include the white helmet, which had been copied from the victorious Prussian armies of 1870. The following is a description of the

original uniform as given in "R.M.C. Dress Regulations, 1876. Pattern of Uniform." (It is interesting to note that the original pillbox as it is known today, was actually a forage cap.)

Tunic:

Scarlet cloth, blue cloth collars and cuffs, single breasted. Collar ornamented round the top with $\frac{1}{2}$ " lace, and tracing of $\frac{1}{4}$ " Russia braid on bottom seam. Cuffs pointed from 2" to 4", and ornamented at top with $\frac{1}{2}$ " lace, and in addition for Cadets of the 1st and 2nd Classes, with tracing of $\frac{1}{4}$ " Russia braid, $\frac{1}{4}$ " above the lace with crow's foot and eye at apex. 8 large buttons in front and 2 at waist behind. Double $\frac{1}{4}$ " round cord, not twisted, with button on shoulder. Skirt closed behind with plait at each side, and lined with white marino. The front, collars, and cuffs and skirt plaits edged with white cloth $\frac{1}{4}$ " wide. Body lined with brown "Italian cloth," or "Farmer's" satin. For 5 ft. 9", skirt 10", with $\frac{1}{8}$ more or less for each 1" under or over that height; brass waist belt hook.

Trousers:

Blue cloth, with a scarlet cloth stripe $\frac{3}{8}$ " wide down each side seam. Pantaloon, blue cloth, stripe as on trousers.

Boots:

Ankle lace, 6 eyelets, 4 hooks.

Over boots, height according to length of leg, square top.

Gloves:

White leather.

Leggings:

Black leather.

Chaco:

Blue cloth, 4" high in front, $6\frac{1}{2}$ " at back, crown 6" long and $5\frac{1}{2}$ " across. Gilt burnished chain $\frac{1}{2}$ " wide, lined with velvet, gilt rose fastenings at side, and lionhead hook at back, gilt cap plate R.M.C. pattern. Plume white horse hair, 6" long, falling $1\frac{1}{2}$ " below top of chaco, gilt ball socket.

(About 1880 replaced with the White Helmet.)

Forage Cap:

Blue cloth with band of $1\frac{1}{4}$ " lace, scarlet cloth welt round the crown, and a gold netted button in centre of top, black leather chin strap, no peak.

(This was replaced by a Cap with peak but at the suggestion of the R.M.C. Club, the original Forage Cap was taken into wear.)

Undress Jacket:

Scarlet cloth, single breasted 28" long from the bottom of the collar behind for a Cadet 5 ft. 9" high with proportionate variation for difference of height. $\frac{1}{4}$ Russia braid double, not twisted, and small button on shoulder, for Sergeant, and scarlet cord, not twisted, for other Non-Commissioned Officers and Cadets. Collar, blue cloth edged top and

bottom, $\frac{1}{4}$ " Russia braid top and bottom. Cuffs, blue cloth, traced and pointed 3" to 5" with $\frac{1}{4}$ " Russia braid terminating in crow's foot and eye at apex, for Cadets of 1st and 2nd Classes, and without braid for Cadets of other Classes. Five gilt buttons (large) down breast, and brass waist belt hook.

Memo.: The above was adopted about 1878, previous to which the Undress Jacket was blue. For your purpose 1876, blue.

Great Coat:

Grey, milled cloth, double breasted, stand and full collar, $4\frac{1}{2}$ " deep, scarlet cloth inside and out. Two rows of buttons down the front, 6 in each row; top buttons 6" apart, bottom ones 4", 4 buttons and back strap on skirt behind; five flat buttons under fly of the collar. Cape of same cloth to button on with strap and buckle in front, and long enough to reach to bottom of cuff of tunic, with four small buttons in front and two hooks on collar.

Cadet Sergeant-Majors may in winter (if they wish) wear, both on and off parade, a grey cloth overcoat, trimmed with grey Persian fur similar to that described for Staff-Sergeants and also grey for gauntlets.

Winter Cap:

Dark grey, Persian lamb, $4\frac{1}{2}$ " high in front, 6" behind, with turn down flap fastened up with a hook and eye. Scarlet top cloth, with gold netted button in centre. Gold embroidered crown in centre of front of cap.

(This cap was replaced in later years with the wedge-shaped caps.)

Winter Comforter

Scarlet wove.

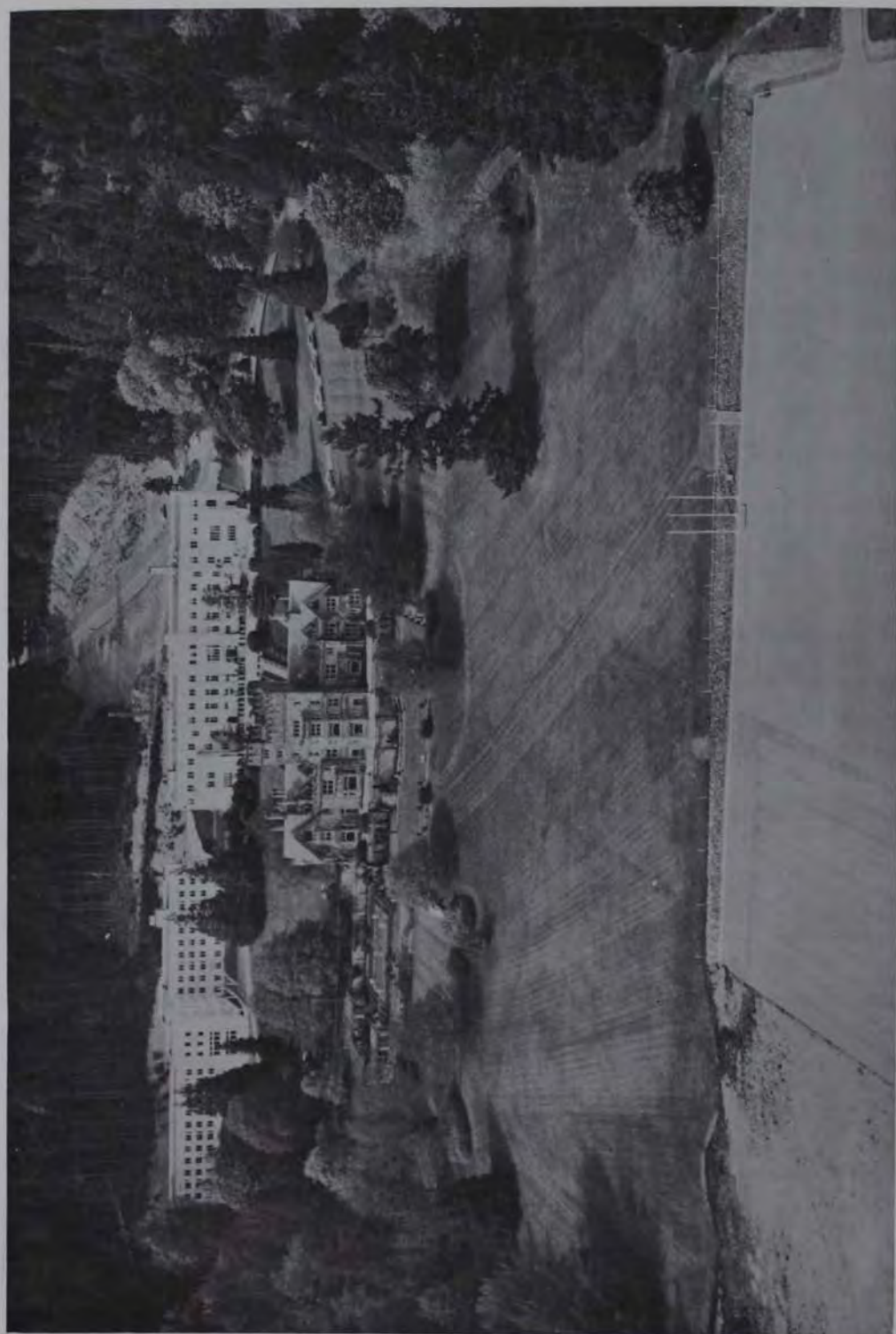
Winter Gloves:

Grey seal skin.

And what of the uniform currently worn by the Cadets of today? Well, for better or worse, it has been slightly changed from the original uniform. Chacos have been done away with, as have the fur trimming on the greatcoats, which are now black instead of grey. The undress tunic is navy blue instead of scarlet, and many of the Cadets do not even know what a comforter is. Winter gloves are now brown instead of grey, and except for the Cadet Officers who wear the original style grey gauntlet, the Cadets wear grey Persian fur mittens. But basically, the uniform today is the same as the uniform worn in 1876 at R.M.C.

Let us remember that it is very important that the Cadets of today uphold the honour and tradition of this uniform, which has already been established by the many great men who have worn this uniform with much pride and distinction.

Note: The information for this article was obtained from the R.M.C. Review, Semi-Century Issue, 1926 (pages 66-68).



SAM SLICK VISITS ROYAL ROADS

By S/C R. W. Munday

Cavanagh: A house must have a solid foundation, but a dinner a soft one.

Antonsen: Every successful enterprise requires three men, a dreamer, a business man and a son-of-a-b——.

Dalley: Only what I drink is mine.

Lytle: Almost anybody will take any amount of it.

Parnetta: Wherever there is authority, there is a natural tendency to disobedience.

Gilbert: So long as there are French mothers, the language will not die.

Harris: Trade follows the advertisement, not the flag.

Kovacs: The best of colleges—a farmer's fireside.

Landry: Barring catastrophe shocking to think of, this colony must to the end of time, be peopled by the Canadian race.

Sykes: Calgary is as big as Chicago, but it is not all built up.

Gibbon: People always laugh at the fool things you try to do unless they discover you are making money out of them.

Kuntz: Ceremony was invented by a wise man to keep fools at a distance.

Gowdy: Somebody once told me if I aimed at the sky I might at least hit a tree. If I wanted to hit a tree, I'd shoot straight for it.

Dallimore: Canadian girls are so pretty, it is a relief to see a plain one now and then.

Saunders: Many a feller looks fat when he is only swollen.

Crane: I am what libraries and librarians have made me.

Hawkins: If your luck isn't what it should be, write a "p" in front of it and try again.

Inglis: The road to the head lies through the heart.

Parkes: Canada's history is as dull as ditchwater and her politics are full of it.

Exley: This life ain't all beer and skittles.

Smith: Canada's vast untouched—and untouchable—resources.

Thomas, V. R.: Doubt not, nor dread, the greatness of thy fate.

Sullivan: Those who drink but water will have no liquor to buy.

Sutherland: Indéed, my friend, you bite very hard.

Takaki: You cannot legislate against geography.

Thomas, R. H.: Advice is sought to confirm a position already taken.

Pellow: Youse farmers want to keep your boys at home. Don't let them go up to the

city; keep them on the farm—the temptation is more pure.

Hopkins: Fat men are good-natured because good-natured men are usually fat.

Lightburn: The daughters were pleasant and pious and more useful than both.

MacDougall: There is hope for a man who can occasionally make a spontaneous ass of himself.

Munday: It is good to have an open mind but be sure it is not open at both ehds.

Grott: Dream of that constable, his name is Fear; he'll be at your heels till you die.

Money: Old fellers always think young ones fools; but young fellers sometimes know old ones is fools.

Porter: The study of history is the playground of patriotism.

Shepherd: You are only what you are when no one is looking.

Thomson: Being great is apparently a very pleasant pastime.

Anderson: Who the devil cares for a monument that actually describes one?

Bernard: What this country wants more than anything else is a fool-killer.

Clark: Bring us the flowers now and you need not bring them to our funeral.

Fallon: Is not excellence in the ranks almost as rare as excellence in the ranks?

Goski: That classic that the world has lost—that little book I never wrote.

Cook: Ah, it looks well from the water.

Elder: It is wonderful what a personal interest the average person has in discussions as to what constitutes genius.

Griffith: An artist has more than two eyes, that's a fact.

Pchojek: Ignorance of the bar is no excuse, and yet some judges continue to get by.

Peter: When one is driven to drink, he usually has to walk back.

Craig: My rule is to let everyone skin his own foxes.

Hammond: What a pity it is that marryin' spoils courtin'.

Buchan: The average man has more than one kick coming—to him.

Widdiffeld: The higher the polish, the more irritated you will find the substance.

Downs: Never read a book, squire; always think for yourself.

Hamakawa: Whatever natur' does least, man does best.

Vardy: Canadians waved the flag but Canada buttoned her pocket.

N.B.—My sincere apologies to Sam Slick, a great comedian, and to Mr. Thomas Chandler Haliburton.

ON LOOKING BACK TO AUSTRALIA FROM CANADA

By J/C R. H. Amaral

Just three years ago I was standing aft on the 24,000-ton "Orion" . . . standing and watching the massive cliffs on either side of the entrance to Sydney Harbour disappear. Yes, these massive cliffs, known as the "Heads," were sinking into the background. How many times had I crossed that harbour from the heart of Sydney to the quiet, peaceful resort of Manly? Now I was leaving it behind. What lay ahead? . . . God only knew!

After 22 days afloat and with Australia, New Zealand, Fiji and Hawaii behind us, we entered the Straits of Juan de Fuca to see for the first time our new home. Where were all the scarlet-coated Mounties and great log booms we had heard so much about? Ah . . . it seems the traveller will always be disappointed. I now realize only too well how mistaken conceptions of other countries may be formed. The too-often asked questions, "What's it like to shoot kangaroos?" or "I guess it's pretty dry down there?" tell one that maybe people think we Australians lived in the middle of the desert amongst the mallee scrub and kangaroos. (I've never actually seen kangaroos in their natural state.)

I seem to remember being told of a small city called Victoria which would be on our way to Vancouver. Ah, yes, the city of Victoria: a small, peaceful city where many people retire and spend their last years. Surely it would be great to live there!! Never did I realize that this quiet, peaceful city

would be my "home" within three years.

After passing two years of "working by day and studying by night" in Toronto, I encountered the name Royal Roads by some strange coincidence. It was not long, then, before I was answering all kinds of unusual questions at 2 P.S.U. (O) (Personnel Selection Unit) in London, Ontario. Well, one thing followed another and here I am!

How I used to dream in those first weeks of my gum trees, the rolling surf and white sandy beaches of good ole Aussie! How I used to dream of the clear blue skies and the warm rain. Alas! I'm afraid I was thinking of these things at the wrong time, for a booming voice would come through to me on the parade square in front of the Grant Block, "What the devil are you thinking of, Amaral?" (I guess no one else stood easy when commanded to come to attention, so I was conspicuous.) Well, anyway, what could I say?

Now, after six months at Royal Roads the thoughts of "home" still come. I still dream of that "wide brown land" and the opal sea, but intermingled with these thoughts come memories of camping in the bushland of Algonquin Park, Ontario, and even glimpses of future days when the ski trail will call.

Strangely enough, something else is creeping in: memories of days not long passed, when "recruits" doubled all over "Hell's 650 acres," not knowing at that time what they were doing nor why they were doing it.

DE OMNIBUS

By J/C L. R. Larsen.

The light fades.

Purple shadows lengthen and blend into the night.

The new moon is hidden among the stars.

Darkness.

The noisy confusion of the daylight world has died.

Gone is the veneer of civilization which covers the lust and passion, the trials and joys, the happiness and fears of the mortal man.

Now I am secure. The soft silky wings of the night angel enfold me like the caressing arms of a lover.

In the dark all things are basic, fundamental; realism is on every hand.

I contemplate the beginning, the mystery of the ylym—the builder of the universe. I contemplate the mystery of that hour when all things shall come to an end. I am Alpha . . . and Omega.

I follow the sweep of the Bear across the northern dome; the upraised staff of the shepherd towers high above my head.

The prose of Milton engulfs my soul. I ponder Hegel, Nietzsche; I walk with Xenophon and Plato.

My thoughts are not of this world. I dwell in a fog-shrouded land beyond the stars.

Eyes unseeing, my sight is in the mind. I watch thus, comforted by the soft darkness, until the rose flush of dawn appears on the eastern horizon.

I sleep.

And the rest is silence.



INTER-FLIGHT SPORTS

INTER-FLIGHT TRACK AND FIELD

By J/C G. A. Tyler

On October 8, 1958, the annual inter-flight track and field meet was held at Sooke Gate Field. Besides the most commendable spirit of the competitors was the effort displayed by all of the flights, especially that of La Salle Flight, which took first place, 12½ points ahead of second place Champlain. Fraser, Champlain, MacKenzie, Cartier and La Salle all placed competitors in first place in the various events. The outstanding indi-

vidual athlete of the afternoon was Jon Pellow, who won the broad jump, the 100-yard dash and the discus throw. Al Pchajek won the mile run and J. Ellis, the one-half mile. G. Brown took the high-jump and the hop-step-and-jump, while D. Fallon copped the shot-put title. D. Smith won the 220-yard dash, with G. Tyler capturing the 440-yard dash. La Salle Flight dominated the relays, winning the 440 and 880-yard relays and coming second in the mile relay to Champlain.

INTER-FLIGHT BOXING FINALS



By J/C R. M. McGimpsey

After a very successful, exciting and interesting series of elimination bouts between various members of the different weight classes, the Interflight Boxing Finals were held on the evening of Friday, 28 November,

1958. The ring was set up in the middle of the gym floor, boxing ring lights were lowered and bleachers were moved into position, transforming a barren, empty room into a scene typical of professional matches.

The first bout, with J/C's Mills and Cole representing the lightweight class, resulted in a win by J/C Cole after some very fast fighting. Next we watched an outstanding fight, with J/C Hemenway suffering three knock-downs and still forging ahead to win out over J/C Wright in the light-welterweight division. Following this match S/C Money put on a tremendous display of boxing skill and counterfighting ability in defeating C-F/L Smith, and thus taking the cup for the light-middle class. Final bout before a short intermission brought L/C J. Pellow against his stubborn opponent, C-F/L Craig, whose outstanding ability to absorb punishment showed up well along with his tremendous endurance. L/C Pellow emerged with the cup representing the light-heavyweight division. The heavy-weight division was next, with J/C Findeis losing out to L/C Thomas, a hard fighter. Next came J/C Mackenzie, who conquered L/C Lytle to cop the welterweight cup. Final bout was in the middleweight class, with C-S/L Pchajek demonstrating his marvellous style to win out over J/C Amaral.

Following the final bout, the presentation of prizes was made to all finalists by Rear Admiral H. S. Rayner, D.S.C., C.D., R.C.N.

THE INTER-FLIGHT CROSS-COUNTRY RACE

By S/C M. J. Grott

The much dreaded Interflight Cross-Country Run took place on the 22nd of October of the first term. Of course the seniors dreaded it much more than the juniors but, despite the handicap of being in poor physical condition, they came in quite well. In fact, the first two positions were taken by seniors, namely C-F/L Pchajek and S/C Widdifield. The next four to come in were Junior Cadets John Ellis, Monty McGregor, Sam Yakamishyn and Bob Spickett in that order. From then on the Juniors and Seniors placed fairly

even. The race, however, was not meant to be a competition between the juniors and seniors but a flight effort as the name signifies. The victorious flight for '58 was Champlain, led by their flight leader, S/C M. B. Sullivan with a time of 25:10. La Salle came in a close second and had the two fastest runners in the race. The remainder of the flights came in as follows: Mackenzie, third; Hudson, fourth; Fraser, fifth, and Cartier in sixth place. The individual speedsters were C-F/L Al Pchajek with a time of 23:51 minutes, S/C Herb Widdifield at 24:15 and J/C John Ellis crossing the tape at 24:40 minutes.

HOCKEY



By J/C L. M. Jensen

At the end of a short hockey season, One Squadron won the inter-squadron championship, narrowly edging out Two Squadron.

One Squadron won the title with continuous display of drive and teamwork together with two well balanced forward lines. J/C Cej played particularly well in goal but One

Squadron unfortunately suffered the loss of S/C Gilbert early in the season.

Two Squadron narrowly missed the league title, losing their final game to Three Squadron. The team had some fine players and particularly sparkled on defence, with J/C MacKay and S/C R. H. Thomas turning in fine performances.

Although Three Squadron finished in last place, they proved to be the cause for Two Squadron not winning the title. They also dealt One Squadron a loss early in the season. S/C Buchan played excellent goal and certainly made Three Squadron a tough opponent.

Following the scheduled play the annual staff versus Cadets and Juniors versus Seniors games were held. The cadets narrowly obtained a two-to-one victory over the staff, while the juniors trounced the seniors seven-to-one, with J/C Laye scoring three goals.

Hockey was enjoyed by all this year and a fine display of sportsmanship and sharp competition with friendly rivalry was exhibited on the ice.

THE INTER-FLIGHT SWIM MEET

By S/C D. Harris

The 1959 swim meet was held on the 20th of March in the Royal Roads swimming pool. The meet consisted of five squadron events, seven flight events, and the diving competition.

The diving competition was won by Fraser Flight with Clegg showing the way. J/C Stocks drew gasps from the spectators when he nearly hit his head on the diving board, but, on the whole, the diving competition was quite keen.

The squadron events were closely contested. Two Squadron managed to come out

on top, thanks to the efforts of Kennedy, Thomas and Hemenway. Three Squadron finished second and One Squadron third.

Champlain Flight came first in the flight events, with Mackenzie Flight second. Perhaps the most surprising upset was Griffith's winning of the breaststroke event against the top members of the representative swimming team. The non-swimmers event was quite humorous and surprisingly fast. Cartier Flight finished in last place but showed lots of flight spirit.

The meet ended with a splash as Macdougall was tossed in the pool.

INTER-FLIGHT SWIMMING MEET

Squadron Events

	150-yd. Medley	50-yd. Freestyle	50-yd. Breaststroke	50-yd. Backstroke	100-yd. Freestyle
No. 1 Squadron	2nd	3rd	1st	3rd	2nd
No. 2 Squadron	3rd	1st	3rd	1st	1st
No. 3 Squadron	1st	2nd	2nd	2nd	3rd

Flight Events

	Diving	25-yd. Free- style	25-yd. Breast- stroke	25-yd. Back- stroke	25-yd. Flutter- board	75-yd. Medley Relay	25-yd. Non- Swimmer	200-yd. Freestyle Relay
Cartier	6th	5th	4th	5th	3rd	6th	5th	5th
Fraser	1st	4th	6th	1st	2nd	1st	2nd	6th
Mackenzie	3rd	2nd	5th	4th	6th	5th	1st	2nd
Champlain	2nd	1st	3rd	3rd	1st	3rd	3rd	1st
Hudson	4th	6th	1st	2nd	4th	2nd	6th	3rd
LaSalle	5th	3rd	2nd	6th	5th	4th	4th	4th

Diving

	Cartier	Fraser	Mackenzie	Champlain	Hudson	LaSalle
Forward Header	14	22	18	17	14	12
Back Dive	8	22	16	21	15	11
Optional Dive	8	22	18	21	14	13
Total	30	66	52	59	43	36

Final Standings

Champlain	1st
Mackenzie	2nd
Fraser	3rd
Hudson	4th
LaSalle	5th
Cartier	6th

INTER-FLIGHT REGATTA

By J/C C. I. Mottershead

At 0915 on Saturday, April 11th, the Inter-Flight Regatta began. There was a light wind from the southwest through the Strait of Juan de Fuca, so that the sailing events promised some action.

The first race was the whaler pulling, in which the Fraser Flight muscle men won out, followed closely by LaSalle and Champlain. Then came the dinghy sailing. The Mackenzie Flight dinghy unfortunately capsized in the southeast corner of the lagoon early in the race, and the superb sailing of S/C Tex Thomas and J/C George Kennedy won the event for Champlain Flight. Next came the

crash whaler race, a chance for experience to show itself, but the winner, Fraser Flight, proved that it takes much more than experience to win this event, since they sported only two naval cadets. Champlain Flight had the misfortune of tiller trouble and had to be disqualified. The final rivalry came in the war-canoe race in whalers manned by fourteen cadets and a coxswain. Hudson and LaSalle Flights spurred each other on for a Three Squadron victory, enabling LaSalle Flight to win the laurels of the day.

Results:

1. LaSalle	19	4. Champlain	13
2. Fraser	18	5. Mackenzie	10
3. Hudson	14½	6. Cartier	7½

INTERFLIGHT TROPHIES, 1958 - 1959

The Wisener Cup (Drill)	Hudson Flight
The Interflight Grand Aggregate Shield (Sports)	La Salle Flight
The Interflight Boat Pulling Trophy	La Salle Flight
The Interflight Soccer Trophy	La Salle Flight
The R. I. L. Annett Trophy for Cross Country Running	Champlain Flight
The E. A. Brown Memorial Trophy for Rifle Shooting	Mackenzie Flight
The Interflight Swimming Trophy	Champlain Flight
The Alfred Atkinson Water-Polo Trophy	La Salle Flight
The Interflight Hockey Trophy	Number 1 Squadron
The Interflight Volleyball Trophy	La Salle Flight
The Interflight Basketball Trophy	Champlain Flight
The Slee Memorial Trophy for Pistol Shooting	La Salle Flight
The Interflight Track and Field Trophy	La Salle Flight
The Interflight Boxing Trophy	La Salle Flight

REPRESENTATIVE SPORTS

REPRESENTATIVE SOCCER TEAM

By C-S/L R. L. Sykes

The fall season started off with a great deal of "soccer-talk." The prospect of a trip to Denver, Colorado, seemed to encourage all of the soccer experts in the cadet wing, and heated discussions resulted.

The discussions, of course, paved the road to the first practice. The response to this practice was overwhelming and this encouraged the P & R.T. Staff. It was apparent from the start that soccer was to be a serious effort this year because a first-class coach was imported from Naden. He was Chief Petty Officer Jack Strachan, and before long cadets with which he intended to mold the squad had been whittled out of the team. Soon these twenty were reduced to fifteen and the team settled down to serious work.

Much time and effort was put into perfecting the various skills of the game. Trapping, passing, dribbling and shooting combined with exercise to improve starting, stopping, breathing and general ball control were all important components of the training schedule.

The season consisted of many games with teams from Victoria College, Venture, Naden, Mount View High and a team representing the Pacific Naval Laboratories. Some games

were won and some games were lost and all in all the season passed with about average success. By some quirk of fate, the Royal Roads staff managed to muster a team and eeked a narrow victory over the squad.

The team developed and "Wee Davie Lightburn," between the uprights, proved to be the star. His untiring efforts were supported by Earl Lytle, Bob Spickett, Norm Inglis, Frank Dvorak, Al Parnetta, Les East, Bob MacDougall, Ian Mottershead, Jerry Stowe, Bob Draper, Claire Castonguay, Bill Voort, Dave Mills and co-captains Ralph Sykes and Ed Exley.

And so off we went to Denver to match our ability against that of the United States Air Force Academy. The game there was the toughest of the year, and though we held them to a scoreless tie in the first half, their endurance at that altitude gave them an advantage in the second half. The game ended in a three to nothing victory for the U.S.A.F. squad.

On our return, soccer took a back seat in the sports vehicle of Royal Roads and occasional games, the highlight of which was a match against a visiting team of Swedish Navy Cadets, brought a close to soccer for 1958-59 at Royal Roads.

RUGGER

By J/C D. B. Mackenzie

The newly formed Royal Roads rugger team enjoyed a very active and promising year. Although none of the cadets had ever played the game before, they progressed rapidly under the skillful and ardent coaching of Professor Dutton and Mr. Privett.

At the first of the season the lack of experience held the team back, but it was not long before the players became more competent in the game and offered a serious threat to any opposing teams. The first few games against the well established Oak Bay Red and

Whites helped the team to become more familiar with the play.

During the first term, Roads lost two close games to Victoria College but succeeded in trouncing the Princess Patricia's Canadian Light Infantry team and Victoria High School. The first game against Venture was also a loss for the blue and white squad. The most thrilling game of the term was against Venture for the Hibbard Trophy. Although the game ended in a scoreless tie, Royal Roads held the majority of the play and was unfortunate not to walk away with a win.

The Royal Roads scrum was unbeatable,



SOCCER TEAM

Standing, left to right—F. A. Dvorak, E. Exley, R. L. Sykes, Lieut. L. L. Greig, N. Inglis, W. Voort, C. I. Mottershead.
 Kneeling—C. L. Castonguay, R. J. Macdougall, R. L. Spickett, D. T. Lightburn, G. L. Stowe, E. R. Lytle, D. G. Mills.



RUGGER TEAM

Standing, left to right—Prof. H. M. Dutton, F. A. Dvorak, G. A. Griffith, D. C. Smith, W. S. Hammond, R. W. Craig, Mr. A. C. Privett.
 Seated—E. G. Dueck, J. C. Gervois, M. W. Cavanagh, J. C. Pellow, L. G. P. Lee, V. R. Thomas, W. J. Taylor.
 Seated on grass—M. F. Peter, R. D. Newman.

and with the strength of Mike Cavanagh, Len Lee and Ernie Dueck and the hooking skill of Rod Newman, usually were successful in getting the ball back to the capable hands of scrum half Mike Peters.

The second term produced much the same

results as the first, although the team gained a great deal more skill and knowledge of the game.

With the experience gained this year the rugby team is looking forward to a very active and successful fall term.

THE BOXING TEAM

By J/C G. W. Brown

The boxing team got off to a good start with training starting immediately after Christmas. The holiday fat soon faded away as J/C MacKay and S/C Thomas paced the team in its daily sprint around the cross-country. Coach L/S Reddin made sure that everyone was sufficiently warm to continue training by a long and hard series of callisthenics.

The team consisted of S/C's Pchajek, Kovacs, Money and Thomas and J/C's Mackenzie, MacKay, Brown and Amaral. J/C McGimpsey, as well, trained with the team

but an unfortunate accident forced him to discontinue this training only a week before Tournament.

The trip to R.M.C. took its toll of pounds, and as a result every fighter was several pounds under his fighting weight. The hard training paid off, however, and Royal Roads placed second in the C.S.C. Boxing Tournament. S/C Money won in the lightweight division as did J/C's MacKay and Brown in the heavyweight division.

The gratitude of the whole team goes to coach L/S Reddin for his invaluable help and coaching.

REPRESENTATIVE SWIMMING TEAM

By J/C J. Findeis

Practicing since last October, the swimming team, though still not up to Sgt. Jones' standards, succeeded in winning two of three meets in which it participated during the college year. In the first one they swam to an easy victory over Victoria College, with the team captain, "Tex" Thomas, setting the pace. The strength of the team came from its free-stylers, Bob Thomas, George Kennedy, Robin Fraser and Ken Hayes, while Tom Rymer and Bob Rutherford competed in the backstroke and Dave Warr and John Findeis in the breaststroke. Jon Pellow and Dave Clegg in the meantime, under the "quiet and easy-going" guidance of the swimming coach, Sgt. Jones, attained a high degree of proficiency in their diving.

In the second meet, against the Naval Training Apprentices, because of the excellent showing of the free-stylers who never lost

a race, and the dogged determination of the remaining members, the final score was 26 points for Royal Roads and 22 points for Naval Training Apprentices.

The final meet proved less successful than the first two as the final standing found Royal Roads third out of five teams. Following this third meet, the training reached its peak as the trip to Kingston became imminent. J/C Tom Rymer replaced J/C Bob Rutherford in the backstroke but otherwise the team was unchanged at departure time. Even the R.C.A.F. North Star could not dampen the high morale and team spirit. However, both R.M.C. and C.M.R. proved to be superior in their swimming abilities.

Since the Canservcol Tournament however, tri-weekly workouts have not stopped and we are confident that next year our swimming team will do its share in restoring the Claxton Cup to its rightful home—Royal Roads.

REPRESENTATIVE BASKETBALL

By J/C G. L. Walt

Under the supervision of Lt. Greig the 1958-59 basketball team began training in mid-December. Unfortunately the team consisted mainly of juniors with varying experience. Only two of last year's members were on the team. Thus it was difficult discovering

individual talents and capabilities.

We won about one-third of our games. On January 9th we were beaten 54-51 by the Navy but the following evening Hass came through with his lay-ups and we won 67-48 against Belmont High. Again on the 10th Vic College won a decisive game 90-54 but



BOXING TEAM

Standing, left to right—R. M. McGimpsey, R. H. Amaral, R. E. MacKay, R. J. Williams, LS J. Reddin,
G. W. Brown, V. R. Thomas, A. W. Pchojek.
Kneeling—R. J. Kovacs, S. A. Money, D. B. MacKenzie.



SWIMMING TEAM

Left to right—G. A. Kennedy, T. C. Rymer, R. A. Rutherford, D. H. Warr, Sgt. T. Jones, R. C. B. Fraser,
D. Clegg, K. S. Hayes, J. H. Findeis, R. H. Thomas.

we rallied the next night and won 53-40 against Belmont. On the 16th the team travelled to Cumberland only to be beaten 42-27. The 17th saw us lose a very close game to the Navy 46-45, which they won in the dying moments of an exciting game. Our hard-hitting first line consisted of "Tall" Thomas, "Dead-eye" Dennis Hopkins, the two veterans, plus Pete Jacob, Dennis McCarthy and Norm Hass. Backed by Bob Kanngiesser, Jim Williams, Dave Smith, Norm Inglis and yours truly, we travelled to Kingston on the 19th, 20th and 21st of February for the annual

Canadian Services Colleges Tournament.

The first day of the tournament we lost a very close game to C.M.R. in a rough game. This put us in second place. On Saturday, the following day, we again lost by a large margin to R.M.C. and ended up on the bottom of the heap.

The 1959-60 team has already begun practising for the Tournament which will be held here at Royal Roads next year. We feel very confident of doing our part to win the Claxton Cup for Royal Roads.

REPRESENTATIVE FENCING TEAM

By S/C R. W. Munday

Fencing got underway as a Saturday morning recreational activity early in September. The rather exacting preliminary exercises cut, which started out as a large club, to five enthusiasts: S/C A. T. Downs, S/C R. W. Munday, J/C J. Ellis, J/C L. R. Larsen and J/C D. L. L. Mulder.

Intensive practice began in January. We spent each sports period learning the fundamentals of advancing, withdrawing, lunging, cut-over and deception tactics under Professor G. S. McCaughey. Early in January we left

snow-blanketed Victoria in favour of Vancouver and competed in foil and sabre against various Vancouver teams at U.B.C. We did not come close to winning the meet but we did learn a great deal from experienced men, some of whom were trained in Hungary. A look at previous editions of The Log will show that our results compare favourably with teams of past years.

The Seattle Northwest Pacific Tournament is in May this year and it is not definite at time of writing whether or not we will be able to go there and compete.

WATER POLO

By J/C D. G. Stocks

This year something new was added to our college sports program. With the facilities offered by the new swimming pool the inter-flight sports schedule was organized to include water polo, a game which few cadets had ever played before. However, it soon proved quite popular and the inter-flight competition became a close struggle.

Early in January it was decided to form a representative team to compete in Victoria with four teams already in existence in this

area. The league consisted of Royal Roads, Victoria College, N.T.S. Collegiate School and Venture. The team, comprised of both Juniors and Seniors, met with success in its first encounters but did not finish the season as well as this early success might have indicated it should.

Early in March the pressure of approaching exams forced the team to stop competing, but with a nucleus of experienced Juniors to start training next fall, water polo should have a longer and more successful season.

THE GYMNASTICS TEAM

By J/C J. S. Hemenway

This year for the first time a gymnastics team was organized and sent to the C.S.C. Tournament at R.M.C. in Kingston. The team, under the expert coaching of Petty Officer Aylward and comprised of C-F/L Craig and J/C's Cole, Calloway and Hemenway, put on a demonstration on the box horse, parallel bars and tumbling mats. Each member had

a compulsory and voluntary routine on each apparatus. The gymnastics event was rather special, as the demonstration served not only to entertain but also to give the other colleges an idea of what type of exercises might be expected next year when gymnastics will be one of the competitions. Royal Roads has all the potential necessary to make a clean sweep of this competition next year and nothing short of the best will satisfy us.



BASKETBALL TEAM

Standing, left to right—N. W. Hass, D. L. McCarthy, R. F. Konnglesser, R. H. Thomas, Lieut. L. L. Greig, R. J. Williams, R. J. Walt, N. Inglis, D. C. Smith.
Kneeling—P. A. Jacob, G. A. Tyler, A. D. Hopkins.



FENCING TEAM

Left to right—Mr. G. S. McCaughey, L. R. Larsen, R. W. Munday, J. S. Ellis, D. L. L. Mulder, A. T. Downs.

THE RIFLE TEAM

By J/C V. Bezeau

The rifle team, under the direction of Staff-Sergeant Dadd, completed another very successful year, losing only one of its competitions. The nucleus of the team was composed of five experienced seniors: Mike Sullivan (the team captain), Don Anderson, Bob Macdougall, Ken Parkes and Gerry Saunders. These were ably assisted by seven Junior Cadets: Vince Bezeau, Dave Carpenter, Roger Miners, Rod Newman, Tony Nichols, Jim Robinson and Dick Waller.

At the time of writing we had won all of our competitions except one; we were in the

lead position in the local shoot for the Phoenix Cup, and we had won several prizes in the "A" Division, Dominion of Canada Rifle Association monthly matches. Our one loss was suffered in the Conservcol Tournament where, firing out of a possible five hundred points, we lost to R.M.C., 489-483.

There can also be no doubt as to the quality of the individual team members, as each one of them won the right to wear crossed rifles. J/C Dick Waller, with an average of over 99 for four targets, won the College Crown Competition, and he, with the other juniors, will no doubt provide a strong base for next year's rifle team.

-REPRESENTATIVE VOLLEYBALL

By J/C G. L. Walt

With Flight Lieutenant Lewis' coaching, this year's volleyball team began practicing for the C.S.C. Tournament in mid-December. The team consisted mainly of seniors. Sets were: Mike Peter, Gerry Saunders and Stu Elder; with Phil Antonson, Dave Porter, Jon Pellow, and the only juniors, Gord Walt and Jim Williams as "spikes." The team won nearly all of several staff games. It also emerged victorious in a round robin with a men's Y.M.C.A. team and a team of Naval Training Apprentices. The games against Y.M.C.A. were won by the required two points while N.T.A. was beaten by 15-3 and 15-8.

The team travelled to Kingston for the annual Conservcol Tournament on February

18th. The first games were against R.M.C. R.M.C. won the first game but we managed to take the second one, 16-14, after a very close and exciting game. Unfortunately, they took another close game and we were forced into second place. The same thing happened against a very tricky C.M.R. team and Royal Roads ended in last place in the tourney.

On March 14th the team was entered in the tournament held at Oak Bay High School. We lost our first games to Victoria High but won 10 consecutive games and placed first in the tournament. Also entered were "A" and "B" teams from Victoria College, Oak Bay High and Venture. Although we will lose the seniors next year we are continuing practices with a junior team and hope to win the Volleyball next year.

CROSS-COUNTRY

By J/C R. J. Wreford

Stand-down weekend for the Cross-Country team was one of the major events of the year, for then the team competed in the annual invitational race sponsored by the University of British Columbia. Several weeks of training had been put in by most members of the team in preparation for this race. On the previous Wednesday we ran against high school competition in Victoria and did quite well.

The report that the team would have transportation and meals paid for by the col-

lege did much to increase the numbers of this devoted group. On Friday, in a final display of effort, training was brought to a close by looking at the scenery, stationary and otherwise, from the windows of an Air Force bus as it drove around the course.

For the race on Saturday, 8 November, 1958, the team was divided into two parts: a junior race entry made up of Senior Cadets Pchajek, Widdifield, Smith and Junior Cadet Ellis; and a Senior team, composed of Senior Cadet Sullivan and Junior Cadets McGregor, Lowthian, Stewart and Wreford. The competition was at once seen to be far above the



GYMNASTICS TEAM

Standing, left to right—R. W. Craig, G. E. Gillespie, PO A. Aylward, R. E. Draper, P. C. Callaway.
Kneeling—J. S. Hemenway, J. W. Cole, M. L. Kochanski.



RIFLE TEAM

Standing left to right—D. M. Carpenter, D. J. Anderson, M. B. Sullivan, S/Sgt. F. S. Dodd, R. P. E. Miners,
R. D. Newman, J. M. Robinson.
Seated—A. J. Nichols, R. J. Macdougall, K. J. Parkes, R. C. Waller, M. V. Bezeau.

standard able to be achieved at a college as small as Royal Roads. It included very strong teams from University of British Columbia, Oregon State, University of Washington and Washington State, as well as an unidentified Australian team. Against these odds it is felt

that the "Roads" team put forth a very good try.

The support of the many cadets who found time to go to the University to watch the race added considerably to the moral of the team as a whole.



CROSS COUNTRY

Standing, left to right—M. C. McGregor, D. C. Smith, H. R. Widdifield, Lieut. L. L. Greig, M. B. Sullivan, J. O. Butler, A. W. Pchajek.

Kneeling—R. D. Stewart, M. J. Grott, J. W. Lowthian, R. J. Wreford, J. S. Ellis.



P.T. STAFF

PO Reddin, CPO Stoddard, PO Aylward



VOLLEYBALL TEAM

Standing, left to right—J. C. Pellow, D. B. Porter, G. L. Walt, F/L K. E. Lewis, R. J. Williams,
P. G. M. Antonsen, G. A. Griffith.
Kneeling—G. A. Sounders, M. F. Peter, W. S. Hammond, J. S. Elder.



SQUASH TEAM

Standing, left to right—W. A. Cook, J. A. M. Landry, S. C. Shepherd, CPO Stoddart, V. R. Thomas,
J. M. Buchan.

CSC TOURNAMENT

CSC TOURNAMENT RESULTS

	RMC	CMR	RR
Volleyball	5	3	1
Basketball	3	5	1
Boxing	5	1	3
Swimming	3	5	1
Shooting	5	3	1

	Boxing		
Won	7	2	3
Lost	1	6	5
Points	15	10	11

Basketball

R.R. 36 - C.M.R. 60

R.R. 37 - R.M.C. 60

C.M.R. 55 - R.M.C. 51

Volleyball

R.M.C. 2 - C.M.R. 0

R.R. 1 - R.M.C. 2

R.R. 1 - C.M.R. 2

Swimming

C.M.R. 39 - R.M.C. 25 - R.R. 16

Rifle

R.M.C. 588 - C.M.R. 584 - R.R. 583

Claxton Cup—R.M.C.

Marshall Memorial Trophy—Cdt. L. Moffat.

Winners of Claxton Cup

1949	RMC
1950	RMC
1951	RMC
1952	RR
1953	RMC
1954	RMC
1955	CMR
1956	RR
1957	CMR
1958	RMC

Winners of Marshall Trophy

1956—R. Burleigh, RMC.

1957—M. A. McAvity, RR.

1958—Cadet L. Moffat, CMR.



NOW AL! NOW!



OOPS! FINGERS!



IT DID

EX-CADETS





GRADUATION, 1958

GRADUATION

1958

It seems regrettable that any occasion which requires so extensive an amount of time to prepare should pass so quickly, but such is the inevitable case, and such was the case for Graduation 1958. However, even though the events came and were concluded with unbelievable speed, the memories of all those participating are most vivid. As the parade hour approached, most held at least some misgivings as to whether leaving was really such a desirable change.

The events of graduation followed a pattern similar to that of previous years, with two days being set aside to complete the many activities planned. Thursday, 8th May, Parents' Day, gave the parents of the cadets an opportunity to meet the faculty and, as well, see the cadets demonstrate their ability in athletics. The P.T. display was divided into four sections consisting of exercises on the box horse and parallel bars, and tumbling and callisthenics exhibitions. Each group in turn gave its display and, while not flawless, the presentation was of the highest quality and was fully enjoyed by those viewing it. Following the display, parents, cadets and staff gathered on the Castle lawn for presentation of athletic and scholastic awards and graduation diplomas. A fitting conclusion was provided for the day with tea on the Castle terrace.

While most important, Parents' Day must take second place to Graduation itself, and when it was realized that the weather was to be excellent, enthusiasm for the occasion further increased. Before anyone realized it

possible, we were standing just off the parade square, awaiting the signal to march on. The seating area around the square was filled and the weather ideal. Each of these factors combined to provide the incentive required for a better than average parade.

At the appointed time the Wing advanced onto the square to the inspiring "My Boy Willie." Following the General Salute, the Cadet Wing was inspected by Vice-Admiral H. G. deWolf, Chief of Naval Staff. The Wing next marched past in squadron column and in column of route, then formed hollow square to hear the address prepared for the occasion by the Minister of National Defence who, due to the demands of his office, was unable to attend as planned. The final portion of the parade came as the Graduating Class fell out and marched past in slow time. As they advanced through the ranks of the new Senior Term, the familiar "Auld Lang Syne" was played and at this instant everyone realized just how much they were leaving behind. Yes, it was over, but not in the memories of those who took part.

The final stage came with the Graduation Ball. For the first time the location was changed to the gymnasium, to overcome crowding which would have inevitably resulted had the quarterdeck been used. As during the previous two days, the HMCS Naden band provided the music which was so greatly enjoyed by everyone. With the close of the ball the stay of many at Royal Roads was over, and with our leaving came only the deepest regret.

Prize Winners and Winners of Academy Awards

The Director of Studies' Cup

Awarded to the Junior Term Cadet who proves himself most outstanding in athletic ability and sportsmanship.

A. W. Pchajek

Subject Prizes

Awarded to Cadets who achieve first place in each Academic Course.

Senior:

Military Studies—W. H. Comstock
English—L. A. Hamilton
History—R. J. Lawson
Physics—A. M. Kipiniak
Chemistry—L. A. Hamilton
French—J. G. M. Smith
Mathematics—V. J. Sokoloski
Engineering Drawing and Descriptive
Geometry—W. R. Cobie
Economics—L. A. Hamilton

Junior:

Military Studies—D. B. Porter
English—D. C. Smith
History—M. B. Sullivan
Physics—E. J. Hinz
Chemistry—E. J. Hinz
French—N. Inglis
Mathematics—R. H. Crane
Engineering Drawing and Descriptive
Geometry—R. W. Craig

Crests

Awarded for Athletic Achievement and Outstanding Sportsmanship.

Officer Cadet F. C. Swain
Officer Cadet K. R. Foster
Officer Cadet P. J. Roseworn
Officer Cadet L. Kuzych-Berezowski
Officer Cadet I. H. S. Roussac
Officer Cadet A. W. Pchajek

The Prize of Honour

Awarded to the Cadet of the Graduating Class who is judged by his fellow Cadets to be the best all round cadet, based on the qualities of leadership and sportsmanship, and on academic ability.

K. R. Foster

The H. E. Sellers' Telescope

Awarded by Mr. H. E. Sellers to the Cadet holding the position of Cadet Wing Commander for the final academic term.

B. D. Hunt

**The United Services Institution of
Vancouver Island Binoculars**

Awarded to the Cadet of the Junior Year who is judged by his fellow Cadets to be the best all round Cadet morally, physically and intellectually.

J. C. Pellow

Nixon Memorial Sword

K. S. Mackenzie

**The Governor-General's Silver Medal
(Second Year)**

Awarded to the Senior Term Cadet who, having passed all subjects, obtained the highest standing in his academic year.

L. A. Hamilton

**The Governor-General's Bronze Medal
(First Year)**

Awarded to the Junior Term Cadet who, having passed all subjects, obtains the highest standing in his academic year.

L. A. Gibbon

**The Lieutenant-Governor of Quebec Medal
(English Speaking)**

Awarded to Senior Term English-speaking Cadet who has made most progress in French.

J. A. White

**R.M.C. Club (Victoria Branch) Award for
Military Training**

B. D. Hunt

The Commandant's Cup

Awarded to the Senior Term Cadet who proves himself most outstanding in athletic ability and sportsmanship.

F. C. Swain

CLASS OF '44

By J. S. Hertzberg

I must beg forgiveness for inaccuracies. The clan is well spread out and news of some is hard to come by.

Congratulations to both John Frank and Pat Nash at having broken the third stripe barrier in January this year. Pat and Jean have been in Ottawa since "Ontario" paid off last Fall. They had their first child in September. John arrived with Joan and their three children in January after a couple of years in "Bonaventure."

Our medical fraternity is well scattered about. Bud Leckey has taken his wife and two boys to London, England, where he is taking post graduate studies in surgery. Peter Heaton has settled in Ottawa with Ann and now has two children. He is making his career in general practice. Don Harrison, with Joan and three boys, is still in Deep River (Atomic Energy of Canada) and has just graduated in internal medicine. Bob Irwin is married and is a surgeon in Winnipeg. Art Leacock, I believe, is practicing in his home town, Olds, Alberta.

The unusual appointments list contains Bud Smith, who has taken Joan with him to California for a couple of years at the U.S.N.'s Supply Centre at San Diego. Ian Macpherson is well in the limelight, having been appointed Equerry to Her Majesty for the Royal Visit. Jill produced their first child last August. Doughie MacLean is on loan to the U.S.N. flying A/S aircraft. His base is Norfolk, Virginia, where Dorothy and their two children are living. We have been represented in Washington for eighteen months by Colin Shaw. Lee had their first child shortly before leaving Ottawa in 1957.

Dany Marcus was sick for some time but I hear he has recuperated and is now on the P.N.O.'s staff in Vancouver, where Audrey and their son are with him. Casey and Helen Cameron are now in Montreal with their six children. Casey is S10 at "Hachelaga." After looking after the Navy's interests in Lynn Creek, B.C., for a couple of years, Sam McNicol is to be supply officer of "Columbia" on commissioning. No doubt he will have Dewey and the family with him in Halifax.

In Victoria we have Bud and Nora Brooks with their three girls, Bud being Captain of "Cowichan." Kelpie Cavanaugh continues to be our highly eligible bachelor and is in charge of the N.D. Training Centre. Fred Henshaw is XO of "Beacon Hill." Naomi has produced him three girls. Don and Judy Joy are in Victoria with a daughter. Don is SO (Plans). Ron and Sheila Manifold with two sons are also there, Robin being XO of "Stettler." Don Sabiston is on the staff of the

newly opened Naval Technical School in "Naden." He and Mary have three boys. Jim Wightman with Marnie and a son, completes our naval quota for Victoria. Jim is SO (Information).

Ted Reynolds, with his family, is in Vancouver, where he is to be seen on TV for CBC. Dun McLaurin is, I believe, still with Yarrows in Esquimalt. Ian Davidson is an architect based at Vancouver, but I hear he spends a good deal of time travelling in Europe. Bev Kaester, teaching in Saskatoon, is active in the R.C.N.(R). He takes his wife and twin girls to Victoria for the summer while he works in the Reserve Training Establishment.

Ottawa is blessed with many of us in the Navy. Stan King has taken time off from being the Hydrographer to assist in planning the naval aspect of the Royal Visit. Elisabeth produced him a boy to balance the three girls. The Communicators have both Al Cockeram and John Gill working for them. Al, with Carm and their son, have been here for about a year, while John and Mary arrived from Halifax several months later. Dick and Edith Niven, with their child, have been here for over a year. Dick is the D. part of the N.D. world. Herb and Iris Rowley, with two children, have been back in Ottawa for eight months. Herb is doing aeronautical engineering for E. in C. Don Gamblin is with the TAS department. He arrived last summer from Halifax with Betty and two kids. The Gunnery World is represented by John Hertzberg, who brought Deborah and their three children from Halifax eighteen months ago. The latest arrival is Ed Wiggs, who has just completed two years on the staff of the S.N.O., Montreal Area. Daphne will join him as soon as he provides her a house.

Of the many natives of Toronto, only two are living there. Jake Howard is well known for his prowess as a barrister. He and Nancy have two children. Jack Chipman is still with Brown Company, for whom he travels quite a bit.

On the East coast Jacques Cote has been Number One of "Restigouche" since she was commissioned. Betty-Ann and the children are in Halifax. Dave Crump has recently returned with Biddy and their three children from "Niobe." He is TAS officer of "Bonaventure." Alec Fox has been Senior Pilot of VF 870 (Banshees) for nearly two years. Lillian and the two kids are in Dartmouth. Dave Jeller moved Jean and his two children to Halifax a year ago from Ottawa when he became D of Cortron 1. Chuck and Shirley Leighton, with their two children, are also in Halifax now after an appointment in Washington. Chuck is the D of Cortron 3.

John Ireland and Tommye have two chil-

dren and are still in Montreal. John is an actuary with Towers, Perrin, Forster and Crosby. George and June Hobart are settled in London, Ontario, with three daughters. George is proprietor of the Utility Paper Company.

Sorry, but nothing at all on Charlie McBride, Don Owen, Bob Stairs or Bob Wood. Dunc Bancroft is reputed to be teaching in Quebec City and Tony Hilliard presumably is still in forestry work in B.C. Fred Sanford is farming near Portage La Prairie.

CLASS OF '48

By H. D. W. Bridgman

It is regretted by the producers of this article that time and circumstances have made the following very brief, possibly incorrect, and above all, totally incomplete. The bias is to the Navy and to the East coast.

J. V. Andrew—Jack is at Naval Headquarters inventing and supervising the manufacturing of a great big wheel.

J. G. C. Atwood—Cluey is also at Naval Headquarters playing with the bigger and better model aircraft called the Banshee.

N. C. Balson—Craig also bides his time at Ottawa.

A. E. Booth—Boots has recently "sunk" a great deal. He is a clearance diver, lives in Victoria, is married and has three children.

H. D. W. Bridgman—Hamish is enjoying that tropical paradise of Halifax and is First Lieutenant of H.M.C.S. Sioux.

M. J. Briere—Marc is married, has countless children, is a prominent lawyer and lives in Montreal.

G. S. Carpentier—Sam is also in Montreal, chasing graft as a chartered accountant.

F. W. Costin—Frank is married, has one child at last count, and is Squadron Navigator on H.M.C.S. Algonquin.

G. A. Cowley—George, having completed a trip around the world in 800 days, returned to the West Coast for a short time with C.P.A. and now is away again. Last rumour—Ottawa in External Affairs.

D. G. Currie—Last heard of drilling oil wells in Saskatchewan.

P. G. Hill—Believed in and around Kingston, wasting away at a tidy 300.

J. H. W. Knox—Another plumber also believed to be in Ottawa investigating bigger engines for smaller ships.

H. P. Labelle—If you want a house designed, see this boy. Having completed the Queen Elizabeth, he is available.

R. Leckie—Leck joined the "Great Lakes" Sailing Association two years ago as Secretary to the Chief of Staff at H.M.C.S. Patriot. Last reports say that he is still there training sailors in cooking.

A. G. Lowe—Algy has returned to us from a two-year stay south of the border with the U.S.N., and is now Communications Officer at H.M.C.S. Shearwater. Signs of home cook-

ing by his li'l ol' wife are beginning to show in his tailor's bills. He lost his heart in the Maritimes and has two "rightsomen good" children to show for it.

R. J. McAllister—Bob was in B.C. for a while a year ago, but it is believed that after the last trouble with the Doukhobors in the Interior, certain people were removed from the province.

R. B. N. McBurney—Bob appears to have disappeared.

E. McCubbin—Ernie, married with two children, is stationed in Ottawa in the office of EE in C of replacing lightbulbs.

D. A. McDonald—"Buttons" is on the West Coast at the TAS Training Centre as Senior Instructor. He is married and has four income tax deductions.

P. D. McIntyre—Peter has been active in Ex-Cadet affairs, having organized the tenth anniversary dinner in Montreal. Further information cannot be released.

F. D. Malloch—Davey chased Eskimoos around for a while with the Northern Affairs Department, Electrical Division (possibly rigging igloos for refrigerators). Now he appears to have returned to Hamilton.

A. F. Morris—Al is TAS officer in H.M.C.S. Restigouche, Halifax. He is married and has two children.

J. F. Miles—Gunder is in H.M.C.S. Saguenay, soon moving to H.M.C.S. Athabaskan as Electrical Officer, based in Halifax.

J. P. Morton—"Wabbit" is TAS Officer in H.M.C.S. St. Croix in Halifax. He is also married with two children. Recently got a new "do-it-yourself" kit and produced a new warren.

V. A. H. Murison—Murch, a Direction Officer in H.M.C.S. Shearwater, is married with three children. He lives in Dartmouth and has joined the building boom with a new dwelling.

F. J. F. Osborne—Ossie may have gone into orbit!

H. H. W. Plant—Hugh is in H.M.C.S. Margaree, also a Navigator. He lives in Victoria, and has two little "seedling Plants."

J. P. Poitras—Jean has been sighted in Montreal, but unfortunately our source of information ceases there.

D. H. Pratt—Denny has become a Bostonian as the R.C.N.'s gift to M.I.T. He is

married and had a couple of children at the last count.

H. L. Price—Eddy appears to have no fixed address.

J. M. Provost—Jenny was still bouncing around the Montreal area when last heard of.

A. C. Rowlands—Since "Black Horse" left Montreal, Al may have taken over.

H. W. B. Sheasby—Bruce has reappeared after many years lost to the R.C.A.F. He flew into H.M.C.S. Shearwater and was sighted. Bruce is a Flight Lieutenant now at Air Force Headquarters. He recently completed the Test Pilot course at Tuxet River.

P. L. Shirley—in absentia—He hasn't appeared on "Front Page Challenge" for over a year.

G. R. C. Sircom—Getting educated at Queens (?). Buck has been sighted on and off during the last few years but his present whereabouts are not known.

W. N. Smith—Smitty may be seen on St. Patrick's Day (or at least heard). He remains another unknown at present.

W. B. Tilden—When last heard of Wally was running a "do-it-yourself" driving outfit in Ottawa. Also, he is married.

J. F. Watson—Jack is TAS Officer in H.M.C.S. St. Laurent and soon will be living in Victoria. He is married and has two young ones.

R. A. Whyte—R.A. is loose in Manitoba as School Relations Officer. Teachers are hereby warned.

J. R. Young—Robbie is First Lieutenant of H.M.C.S. Outremont and is married with four flourishing children and a dog. If the latter doesn't get you the former will. Robbie is now an expert on polar travel.

As you can see, it is very brief, mostly guesswork and mainly fiction. Apologies are made to those about whom much detail cannot be found. Next year may prove more fruitful.

Last year was our tenth anniversary and those in the Navy qualified for pension; apart from that and a few grey hairs, nothing outstanding has happened.

CLASS OF '53

By J. S. Upton

I have had to do a bit of scurrying to find information for this article and I'm afraid that some of it may be outdated. Nevertheless, having made my apologies, I will venture forth as requested.

Last year while in the Edmonton area I met John Law, his wife and son. John was then the Air Force equivalent to Q.M. at R.C.A.F. Station Namao. Also met Bob Thompson and his wife. Bob was with a transport squadron flying out of Edmonton. Bill Jopling is another advocate of married life, and is now working for a pipeline company in Edmonton. It is rumored that Don Duncan is still flying in the Alberta area, but I was unable to track him down last year. Hank McKeen is also supposed to be travelling for a meat firm in the area, but this too I was unable to confirm.

While I was in hospital last year, Phil Cheevers dropped in. Phil, now married and a father, was on his way to Camp Shilo to join the Arty School in some obscure role.

Last September I met Roger Devine in Montreal. Roger had just joined the Works Company there and was adjusting to the new job and bilingual atmosphere.

Dick Ursal and Jack Falkins are still with the Air Force in Europe, I believe. When speaking of Europe, I must include the sad news of Ross Raymond's death. Ross was involved in an aeroplane crash last August. All the class join me in expressing our deepest

sympathy to his wife, family, parents and relatives.

I hear that Stan (Sammy Walrus) Wallace is now married and still happily working for D.H.O.

Jimmy Priebe, when last seen, had received his Master's degree in Chemical Engineering and was heading Ottawa way for a position with a large chemical company.

I have lost track of most of the Navy types. Fred Jardine and Norm Haslett are still practicing Ohm's law within the Navy's hallowed circle.

Guy Lessard is supposed to be with the R22eR in Quebec area, but I have been unable to find him. Speaking of Quebec, I was rather surprised to find Paul Godbout ('50-'52) acting XO at H.M.C.S. D'Iberville. As for myself, I've transferred to C. Int. C. and am now Area Intelligence Officer for Eastern Quebec area. Some of the term may be interested in learning that I have now graduated to the "big car" field. The Messerschmitt (3 wheels, 500 pounds) has been retired in favour of a Volkswagen. Incidentally, I am still one of our shrinking number of bachelors.

Fraser Isbester, Lou West and Ralph Howsam are still with the Arty, I believe. Lou married after graduation from R.M.C. and Fraz took the step after our year at Queens. I believe Ralph is married, but have not been able to confirm this information.

Earl Sinnett was still a flying instructor near London, Ont., when last heard from. Earl and Terry have a small cocker spaniel

named "Montgomery." At last report Earl was thinking of trading his Olds '58 for a newer, lower, slinkier model. Must be nice to have flying pay!

Scotty Price is still flying for TCA and now apparently breaks the monotony of trans-Atlantic flights with an occasional trip to the West Indies.

Harry Jonas transferred to R.C.E. about

two years ago and was last heard of in Camp Chilliwack.

Cliff (Shakey) Shook won an Athlone scholarship a year or so ago and is perhaps still in England. In any event, reports have it that Cliff has not lost one iota of his old wit and quick rapport.

Rick Bolli joined the ranks of married men about a year ago, but was torn from his bride for sea duty.

CLASS OF '54

By M. Sugimoto

C. G. Bale—Gord is now at R.M.C. in the Economics Department seeing what it looks like from the other side of the podium. He plans to tour Europe this summer.

A. K. Beare—Arch and family are now in Germany with 1 R.C.H.A. as Assistant Adjutant and former Gun Position Officer. At the end of each day Arch clears his in-basket with the aid of an old Howitzer.

R. Bethel—Dick is still single and is now serving with the Canadian Guards in Germany.

A. C. Brown—Archie is with 2 Field Squadron, R.C.E., Camp Gagetown, N.B.—right army, wrong coast.

R. G. Burnie—Bob was married to Marion Agnew in Prince Rupert last June, and he is navigating CF-100's at Comox, B.C.

F. Carson—Frank was last heard of from a radar station in Northern Quebec, where he is a Fighter Controller for the R.C.A.F.

H. F. Champion—Demers—Frank and Denise now have two youngsters, their latest being a Frank MK II version. Frank is with the R.C.H.A. in Gagetown, N.B.

L. R. Creelman—Len is with 1 R.H.C. in Aldershot, N.S.

T. A. Croil—Tom has completed his Masters Degree at U.B.C. and is now unofficial O.C. Distilleries at the Canadian Chemical Company in Edmonton.

E. B. Fletcher—Earl took that stroll down the aisle last year and is now happily married and living in Ottawa, where is navigating for 410 Sqn. in CF-100's.

J. R. Fournier—Jack was married (October, '58) to Elaine Graham in Winnipeg, where he is instructing at the R.C.A.F. Navigation School.

N. S. Freeman—Max has been a navigator with 440 AW (F) Sqn., but has recently transferred to the CE section on the base at 3 Wing in Germany.

W. C. Fraser—Bill and his wife now have a boy and a girl out in Comox, where Bill flies CF-100's.

H. Graham—Hal was last heard to be navigating a VIP aircraft over in Egypt,

D. M. Gray—Don is now a Lt. (CE) R.C.N. in Ottawa, where he is project engineer for Northern Naval Stations.

F. A. Gunter—Fred returned from his sojourn in Paris last July and has taken a position with Glenview Park Collegiate in Galt, Ont., teaching English and French. During the Christmas holidays Fred returned to Paris to see a colonel about a daughter, Emmanuelle Meltz, and they are now happily married and living in Galt.

F. A. Hlohovsky—Frank, "the terror of the Cyprus Hills," is back at R.C.E.M.E. School in Barryfield.

D. H. Hook—Dave is winding up his tour at Macdonald, Manitoba, where he has been teaching young bird-men to fly T-birds. He was recently engaged to Merle Chamberlain.

J. S. Ink—Johnnie is having a ball at 2(F) Wing, Grostenquin, Franke, writing Scripto all over the sky with something like eight tons of Sabre strapped to his back.

M. C. Johnston—Murray and his wife are now in Winnipeg, where Murray is with the 213 Workshop, R.C.E.M.E.

W. Johnston—"Bongo" is now with the CE Section at R.C.A.F. Station Uplands after a short tour on the CF-100 OTW in Cold Lake.

R. I. Kingham—Ian is resident engineer at AASHO Road Test for the Canadian Good Roads Association in Ottawa, Illinois. He took this post after working on the Selkirk Mountain section of the Trans-Canada Highway.

W. S. Laidlaw—Willy "Lunys Lunys" is still with the Imperial Pipeline Company in Edmonton. Our eternal bachelor is free-lancing about the city with his gold dust twin, "Witch" Girling.

P. D. Manson—Paul and family are in Europe, where Paul is driving twin blowtorches for 440 AW(F) Sqn. at 3(F) Wing.

P. Moody—Elusive Paul is in London, Ont., dreaming up bigger and better things in an architectural firm.

J. Murphy—Jerry is instructing space students at Penhold, Alta. He and his wife are the proud parents of a baby boy.

R. J. Neill—Roger is stirring the stick in a T-33 at MacDonald, Manitoba, for his student pilots.

F. J. Norman—"Sport Car" Frank is now with the 2 R.C.R. in London, Ont.

D. S. Oaks—Steve had hopes of becoming an eternal undergraduate majoring in bridge at U.B.C., but we suspect that the faculty shut down for further studies with Goren, for Steve is now with the B.C. Power Commission in Victoria.

H. F. Pullen—Hugh has completed his jump course and is now with the 2 R22eR, Camp Valcartier, Quebec. Hugh would like lighter equipment so that he can complete at least two bars of music on his bagpipes before hitting the ground.

C. S. Robertson—Charlie is with the R.C.S.M.E. in Chilliwack in B.C.

R. P. D. Round—Robin married Olive Deasty in October and they now reside in Victoria, where Robin works as Field Engineer for the B.C. Power Commission.

R. C. Rud—Bud and his wife, Barb, are in Penhold, where he is managing not to overstress the structure of those Harvards.

J. E. Rymer—"Co-Co" is also at Penhold, where he is chalking up multi-hours of instructional flying time.

J. Shantara—Jim is a Radar Special Projects Engineer for the D.O.T.

A. D. Sherwin—Al is another lucky sword driver in Europe with No. 3(F) Wing.

W. J. Shewaga—Bill has left the flying business to study as a Telecom Officer in Clinton, Ont. Bill and his wife, Ester, have a son born last September.

F. D. Simpkin—Fred is lecturing at R.M.C. in Civil Engineering. Fred is also one of the senior football coaches for the College.

P. S. Simpson—Pete is working in St. Catharines, Ont., as Field Engineer for Thompson Products Ltd.

I. J. H. Smart—Ian is the Accounts Officer for the 1 Canadian Guards in Petawawa.

R. P. Smith—Bob is the Assistant Adjutant for the 2 P.P.C.L.I. in Griesback, Alta.

M. C. Stewart—Murray is the Intelligence Officer for the 2 P.P.C.L.I., also at Griesback. The P.P.C.L.I. at Griesback are definitely in good hands.

D. S. Spooner—Dave married Rosalie Gardner last August and they are in Toronto, where Dave works for the Prudential Life Insurance Company.

K. F. Stubbings—Ken is navigating for the 416 Sqn. at St. Huberts. He is often heard cheering the Native Sons from behind his glass of suds.

M. Sugimoto—Sugi has but one task left before being temporarily unemployed. He is digging a huge grave beside the flight test hangar at Malton to bury a dead horse that has been flogged to death recently.

A. C. Wade—Art and wife Bev are still in Chilliwack, but now with a family.

J. T. Whitely—John and wife are now in Manchester, England, where John is employed by a British industrial firm.

J. R. Wigmore—John is with the Black Watch in Aldershot, N.S.

R. Wilbur—Dick was last heard of working for the Bell Telephone Co. in Montreal. He has yet to master the Yogi headstand atop a well-croscoted telephone pole.

Ted White—Ted is out over the blue waters with Uncle Sam's Navy doing carrier operations with the Fleet Air Arm.

C. A. Olson—Charlie is still single. After leaving Royal Roads in 1954, he attended St. Francis Xavier University in Antigonish and is now a chartered accountant student with Price, Waterhouse & Co. in Toronto. He will be touring Europe with Gord Bale this summer.

D. J. Brown, R. Mace—Both are on the Marine Specialist course in Plymouth, England, and are also busily occupied raising families.

P. Tisdall, C. Gunning—Both are also at Plymouth on the Air Specialist course.

CLASS OF '55

By C. D. Jacobs

The following are a few personal and professional informative notes about the first three Belgians ever to train with the R.C.N.

Jef De Wilde is still unmarried, living at Wanzele, Molenstraat, 23. He served as a watchkeeping officer aboard Algerines, a duty which brought him the cold of Iceland as well as the heat of the Belgian Congo. He was recently appointed to an M.S.O. as "Ops," which includes the navigation and communications departments. Jef was promoted Lt. J.G. at the end of last year.

So was Paul Segers, but on July 8, 1958, he had already been promoted as head of the

family when he married Maria Lorre. They are now living in Ostend, Blauwe Kastelstraat, 92. Paul was first lieutenant on an M.S.C. for one and a half years; now he is a watch-keeping officer aboard an Algerine.

Camiel Jacobs was married on February 5, 1958. On November 29 a little Margareth joined the young family, which lives in Ostend, Hyacinthenlaan, 7. After a short turn-around trip to Banana aboard an Algerine, Camiel was appointed first lieutenant to an M.S.C. and changed this job recently for a similar one to Jef's aboard another M.S.O. He was equally promoted to Lt. J.G. last December.

CLASS OF '57

By O/C G. D. Simpson

Forty-five of the original class are in their final year at R.M.C. with three more following one year behind. We have lost contact with a few of our class, but all the rest seem to be thriving. Dave Leslie and Bill Molnar have donned the kilt and are in the Black Watch, while Gil Bellavance and family have crossed to Germany with the R. 22e R.

Hal Sherwood, Kyo Ujimoto and Bob Golds are all flying with the R.C.A.F. Hal is in 3(F) Wing in Europe. Kyo was last heard of at Summerside, P.E.I., with Maritime Command.

Kit Anstis is now at University of Toronto.

Kit is letting the engine of his M.G. cool while he drives a taxicab. Roy Engel is somewhere between Mexico City and Kingston and travelling north.

Hugh Dick has worked his way up to manager of Woolworth's in Vancouver, while Roy Culbertson is in a florist's in Barrie.

John Foster is preparing to solve the problems of the world by taking foreign affairs at University of Alberta. Gary Hartt is believed to be at University of Saskatchewan.

John Cable is a budding chartered accountant in Kingston, while Jon Williams, now with a healthy family of two, is in Toronto in the offices of Mutual of Omaha.

CLASS OF '58

By O/C W. L. Wharton.

Graduation, summer training, and leave for some of us have been the sequence of events for the Class of '58, the majority of the members now being occupied in third year year at R.M.C. Engineering, General Science, Arts—we have infiltrated everywhere and have become an integral part of the R.M.C. Cadet Wing.

Upon looking at the lists of College representative teams, one finds the Class of '58 very well represented. Starting the Fall Term was track and field. Two of our stalwart runners, Pete Scholz and Denny Burningham, distinguished themselves; Denny also proved to be the fastest man in the Wing by winning the Cadet Wing Harriers with Pete a close third.

Representative Football was in the light next. Such names as Pete Spittal, Cliff Swain, Ian Roussac, Bob Billings, Jeff White, Mel Brown, Don MacLeod, Walt Moore, Leo Gervais, Jim Cooling, Walt Cotie and Bob Brett appeared on the lineups.

The R.M.C. Representative Soccer Team was the first College team to win its league this season. Former R.R. types include Bill Sharkey, Ken Foster, Lou Kuzych, Ken MacKenzie and Bill Claggett. It may be men-

tioned that the team never suffered a loss all season.

Representative Basketball started early this year, the team being in the Ottawa-St. Lawrence Conference. A fourteen and four record placed the Redmen in second place; two games only were lost in league play. Three of R.R.'s '58 team, Gary (Jake) Jacob, Wayne Wharton and Pete (Rosey) Roseworn again showed their faces. At last count, Big Rosey was the second highest scorer in the league.

The College Hockey Team, presently second in their league, boast some '58 class members: Mel Brown, Ken Foster and Roger Reid-Bicknell. Brown has been one of the spark plugs for the team, figuring very highly on the team's scorecard. Good luck at West Point, fellows.

The Representative Swimming Team has a backbone of three R.R. fellows—Einar (Ike) Voldner, Pete Scholz and Chip Comstock. These three gentlemen teamed together to win the medley in a recent inter-college meet. Ike Voldner now holds the R.M.C. pool record for the fifty-yard breaststroke.

Something new this year for the R.R. types is the Gymnastics Team: members include Brian Weatherhead, Don Cundall, John

Cal Connor is now attending Queen's University, and was their able quarterback on the Senior Football Team.

Wayne Roberts is married (the first of our class to go down!) and is now living in Vancouver.

Cris Evans has completed a tour of Europe and is presently at the University of Alberta.

Gil Mousseau is at Western studying Commerce.

Art Fennel may be seen roaming the Dominion at large in a '54 Ford.

On behalf of Class of '58, I would like to extend our best wishes to the Graduating Class, the Class of '59.

(Sam) Houston, Ken Sinclair, Steve Newroth, Lou Kuzych, Walt Cotie and Carl Hunter. They are hard at work each morning at 0630 as well as during sports periods.

The R.M.C. Rifle and Pistol Teams, who

are always highly rated, include as sharpshooters: Lou Kuzych, Tom Clarke, Jahn Bird, Denny Burningham, Bruce Waters, Chip Comstock and Charlie Hooker.

On the academic side of our training, Larry Hamilton was presented with an award for his first place in academics while attending Royal Roads.

This summarizes the various members of the Class of '58, who are at R.M.C.; now for the "wanderers" of our class.

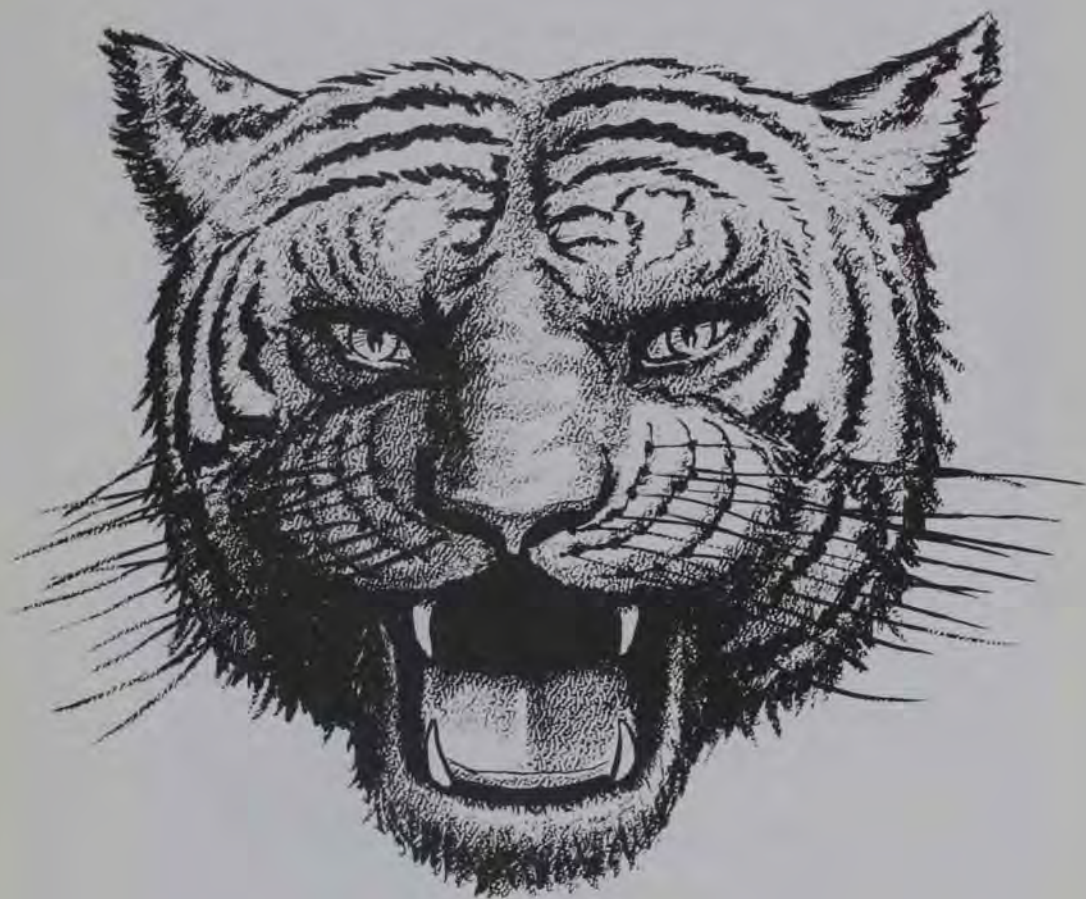
F/C George Ellerbeck is now flying Harvards at Penhold, Alberta.

F/O Arnold Nordick is on Sabres, having been just transferred to Europe.

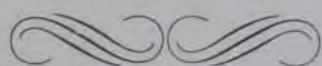
F/O Dave Morrow is flying CF-100's out of Cold Lake, Alberta.

S/Lts. Mike Hodgson and Tony Bennett are both at Halifax with the Senior Service.

CONGRATULATIONS



CLASS OF 1959



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